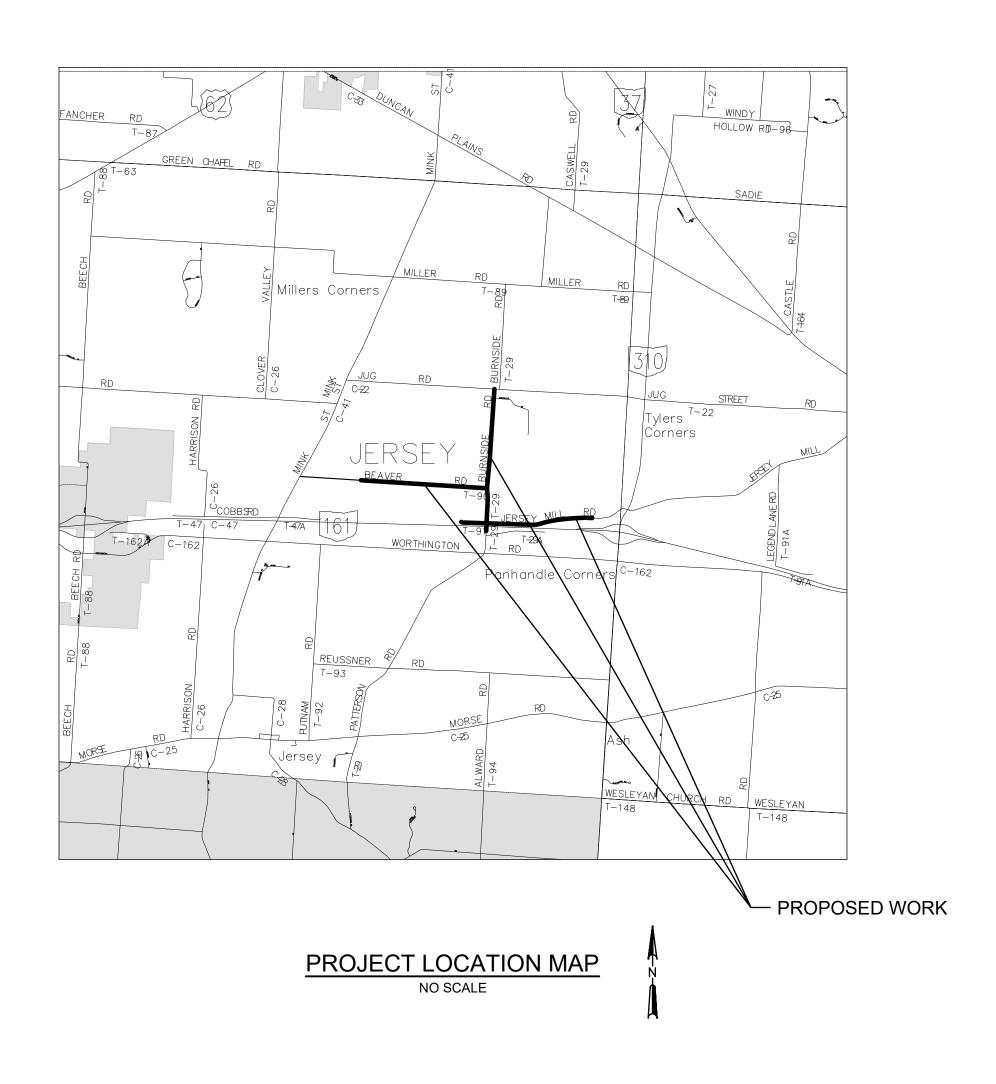
STATE ROUTE 161-MINK STREET WATERLINE EXTENSION

FOR THE

LICKING REGIONAL WATER DISTRICT LICKING COUNTY, OHIO

AUGUST 2025





DRAWING LIST

SHEET NO. SHEET TITLE

1 TITLE SHEET

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4 SHEET INDEX

5-11 PLAN & PROFILE SHEETS

Strand Associates, Inc. 500 Neil Ave, Suite 110 Columbus, OH 43215 614-835-0460 www.strand.com

AREA MAP
NO SCALE

CERTIFIED BY:

HAROLD P. KARNES, P.E.
OHIO REGISTRATION NO. 77895

APPROVED BY:

CHRISTOPHER GILCHER
OPERATIONS DIRECTOR
LICKING REGIONAL WATER DISTRICT



CONTRACT 1-2024

100% DESIGN DRAWINGS

NOTIFICATION

THE CONTRACTOR SHALL NOTIFY THE JERSEY TOWNSHIP TRUSTEES AT LEAST 48 HOURS PRIOR TO THE ANTICIPATED START OF CONSTRUCTION.

JERSEY TOWNSHIP TRUSTEES: (740)-927-0331

UTILITIES

LISTED BELOW ARE ALL UTILITIES LOCATED WITHIN THE PROJECT CONSTRUCTION LIMITS TOGETHER WITH THEIR RESPECTIVE OWNERS.

(614) 496-3651

(330) 682-1659

MARATHON PIPELINE

MICHAEL PRESTON

HEATH, OH 43056

(740) 344-2102

855 KELLER ROAD

ELECTRIC
AEP OHIO
PAUL PAXTON
777 HOPEWELL DRIVE
HEATH, OH 43056
(740) 348-53322

THE ENERGY COOPERATIVE ETHAN BODE 1500 GRANVILLE ROAD NEWARK, OH 43055 (740) 344-2102

ODOT DISTRICT

RANDY HITE 9600 JACKSONTOWN ROAD SE JACKSONTOWN, OH 43030 3323-5129

JERSEY TOWNSHIP
ROB PLATTE
1481 MINK STREET
PATASKALA, OH 43062
(740) 927-0331

TELEPHONE
CHARTER COMMUNICATIONS
TONY ADAMS
3760 INTERCHANGE ROAD
COLUMBUS, OH 43204

GAS
ASPIRE ENERGY
CHERYL LEMON
300 TRACY BRIDGE ROAD
ORRVILLE, OH 44667

(740) 258-4911

THE ENERGY COOPERATIVE ETHAN BODE 1500 GRANVILLE ROAD NEWARK, OH 43055

THE CONTRACTOR SHALL NOTIFY OUPS (1-800-362-2764) AT LEAST 48 HOURS PRIOR TO THE START OF CONSTRUCTION. NON-MEMBERS MUST CALL DIRECTLY.

UTILITY LOCATIONS: THE LOCATION OF UNDERGROUND UTILITIES SHOWN ON THE DRAWINGS ARE AS OBTAINED FROM THE OWNERS OF UTILITIES AS REQUIRED BY SECTION 153.64 ORC. LOCATIONS AND ELEVATIONS OF EXISTING UNDERGROUND UTILITES AND STRUCTURES SHOWN IN THE DRAWINGS ARE APPROXIMATE ONLY. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO DETERMINE THEIR EXACT LOCATION AND ELEVATION WHEN WORKING IN THEIR VICINITY.

ALL ROCK EXCAVATION, AS DEFINIED IN THE SPECIFICATIONS, SHALL BE CONSIDERED INCIDENTAL TO OTHER ITEMS OF WORK. COST FOR ROCK EXCAVATION, IF ENCOUNTERED, SHALL BE INCLUDED IN THE PRICES BID FOR OTHER ITEMS OF WORK.

DISTRICT GENERAL NOTES

- 1. GENERAL: THE CURRENT LICKING REGIONAL WATER DISTRICT CONSTRUCTION AND MATERIAL SPECIFICATIONS TOGETHER WITH THE O.D.O.T. CONSTRUCTION AND MATERIAL SPECIFICATIONS INCLUDING ALL SUPPLEMENTS THERETO IN FORCE ON THE DATE OF CONTRACT, SHALL GOVERN ALL MATERIALS, METHODS OF CONSTRUCTION AND WORKMANSHIP INVOLVED IN THE IMPROVEMENTS SHOWN ON THESE PLANS, EXCEPT AS SUCH SPECIFICATIONS ARE MODIFIED BY THE FOLLOWING SPECIFICATIONS OR BY THE CONSTRUCTION DETAILS SET FORTH HEREIN.
- 2. WORK LIMITS: THE CONTRACTOR SHALL CONFINE HIS ACTIVITIES TO THE PROJECT SITE UNDER DEVELOPMENT, EXISTING RIGHTS—OF—WAY, CONSTRUCTION EASEMENTS AND PERMANENT EASEMENTS, AND SHALL NOT TRESPASS UPON OTHER PRIVATE PROPERTY WITHOUT THE WRITTEN CONSENT OF THE PROPERTY OWNER.
- 3. MISCELLANEOUS WORK: ALL ITEMS OF WORK CALLED FOR ON THE PLANS FOR WHICH NO SPECIFIC METHOD OF PAYMENT IS PROVIDED SHALL BE PERFORMED BY THE CONTRACTOR AND THE COST OF SAME SHALL BE INCLUDED IN THE PRICE BID FOR THE VARIOUS RELATED ITEMS.
- 4. FIELD TILE: ALL FIELD TILE AND OTHER DRAINAGE TILE BROKEN DURING EXCAVATION SHALL BE REPLACED TO ITS ORIGINAL CONDITION AS DIRECTED BY THE DISTRICT REPRESENTATIVE.
- 5. SURPLUS EXCAVATION: THE CONTRACTOR SHALL PROPERLY DISPOSE OF ALL SURPLUS EXCAVATION.
- 6. EXISTING UTILITIES: THE INFORMATION SHOWN CONCERNING EXISTING UTILITIES IS NOT REPRESENTED, WARRANTED OR GUARANTEED TO BE COMPLETE OR ACCURATE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO PHYSICALLY LOCATE AND VERIFY IN THE FIELD, ALL UTILITY LOCATIONS AND ELEVATIONS, WHETHER SHOWN ON THE PLANS OR NOT, PRIOR TO THE BEGINNING OF CONSTRUCTION OPERATIONS. THE CONTRACTOR SHALL SUPPORT, PROTECT, AND RESTORE ALL EXISTING UTILITIES AND ASSOCIATED ITEMS TO THE SATISFACTION OF THE UTILITY OWNER. THE CONTRACTOR SHALL NOTIFY ALL UTILITY OWNERS, IN ACCORDANCE WITH SECTION 153.64 OF THE OHIO REVISED CODE, AT LEAST FORTY—EIGHT (48) HOURS PRIOR TO THE BEGINNING OF ANY WORK, TO REQUEST UTILITY LOCATIONS, TO COORDINATE THE WORK, AND TO KEEP THE UTILITY OWNERS APPRAISED OF THE CONSTRUCTION SCHEDULE AND REQUIREMENTS UNTIL ALL WORK IS COMPLETED. NOTICE SHALL BE GIVEN TO THE OHIO UTILITIES PROTECTION SERVICE (800—362—2764) FOR THE MEMBER UTILITIES.
- 7. SAFETY OF CONSTRUCTION: COMPLIANCE WITH THE OCCUPATIONAL SAFETY AND HEALTH ACT OF 1970, AS AMENDED, AND APPLICABLE OSHA REGULATIONS IS REQUIRED OF ALL CONTRACTORS ON THE PROJECT. EACH CONTRACTOR AND SUBCONTRACTOR IS RESPONSIBLE TO IMPLEMENT, MAINTAIN AND SUPERVISE ALL SAFETY REQUIREMENTS, PRECAUTIONS AND PROGRAMS IN CONNECTION WITH THE WORK.

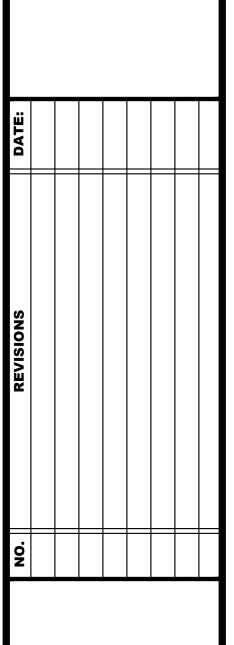
- 8. NOTIFICATION: THE CONTRACTOR SHALL NOTIFY THE DISTRICT (740-927-0410) FORTY-EIGHT (48) HOURS, EXCLUDING WEEKENDS AND HOLIDAYS, PRIOR TO THE START OF CONSTRUCTION OF WATER AND/OR SANITARY SEWER UTILITIES. WHEN CONSTRUCTION IS SUSPENDED FOR MORE THAN SEVEN DAYS, THE CONTRACTOR SHALL NOTIFY THE DISTRICT FORTY-EIGHT (48) HOURS, EXCLUDING WEEKENDS AND HOLIDAYS, PRIOR TO RESUMING CONSTRUCTION. CONSTRUCTION SHALL NOT START PRIOR TO THE REQUIRED NOTICE BEING GIVEN.
- 9. CLEAN-UP: THE TRACKING OF MUD, DIRT OR DEBRIS UPON ANY PUBLIC ROADWAY IS PROHIBITED AND ANY SUCH OCCURRENCE SHALL BE CLEANED UP IMMEDIATELY BY THE CONTRACTOR. THE CONTRACTOR SHALL CLEAN UP ALL DEBRIS AND MATERIALS RESULTING FROM THE CONSTRUCTION OPERATION AND RESTORE ALL SURFACES, STRUCTURES, DITCHES, AND PROPERTY TO ITS ORIGINAL CONDITION AND TO THE SATISFACTION OF THE DISTRICT.
- 10. PERMITS: THE CONTRACTOR SHALL OBTAIN ANY AND ALL NECESSARY PERMITS PRIOR TO BEGINNING CONSTRUCTION. ALL WORK SHALL BE PERFORMED IN ACCORDANCE WITH THE APPLICABLE FEDERAL, STATE AND LOCAL REGULATIONS AND REQUIREMENTS.
- 11. CROSSINGS: ALL WATER MAINS AND SERVICES SHALL BE CONSTRUCTED WITH AT LEAST 10 FEET HORIZONTAL SEPARATION AND AT LEAST AN 18-INCH VERTICAL SEPARATION FROM ALL SANITARY AND STORM SEWERS.
- 12. TRAFFIC CONTROL: WHERE IT IS ANTICIPATED THAT THE UTILITY WORK WILL CLOSE A STREET, THE CONTRACTOR SHALL INFORM THE RESIDENTS TO BE AFFECTED, COUNTY SHERIFF'S OFFICE OR LOCAL POLICE DEPARTMENT, FIRE DEPARTMENT, THE DISTRICT, APPLICABLE SCHOOL DISTRICT, AND OTHER APPLICABLE ENTITIES AS TO THE EXTENT, NATURE, AND TIME OF THE ANTICIPATED WORK. ADEQUATE LIGHTS, SIGNS, AND BARRICADES SHALL BE USED, AS REQUIRED IN THE O.D.O.T. CONSTRUCTION AND MATERIAL SPECIFICATIONS AND OMUTCD, TO SAFEGUARD THE TRAVELING PUBLIC AT ALL TIMES. ALL TRENCHES SHALL BE BACKFILLED OR SECURELY PLATED WITHIN THE PUBLIC RIGHT—OF—WAY DURING NON—WORKING HOURS.
- 13. CURB MARKINGS: THE DISTRICT REQUIRES THAT THE LETTERS "WV" FOR WATER MAIN VALVE, "W" FOR WATER SERVICE LINE, "S" FOR SANITARY SEWER SERVICE LINE, AND "MH" FOR SANITARY SEWER MANHOLE BE EMBEDDED IN THE CONCRETE CURB. FOR STANDARD CURBS, THE LETTER IS TO BE LOCATED IN THE CENTER ON THE TOP FLAT PORTION OF THE CURB. FOR ROLLED CURBS, THE TOP OF THE LETTER IS TO BE LOCATED ON THE FACE OF THE CURB WITHIN ONE AND ONE—HALF (1–1/2) INCHES OF THE CREST. THE LETTER IS TO BE AT LEAST TWO (2) INCHES WIDE, THREE (3) INCHES HIGH AND ONE QUARTER (1/4) INCH DEEP. THE LETTERS "WV" AND "MH" ARE TO BE LOCATED ON THE CURB IMMEDIATELY ADJACENT TO ALL WATER MAIN VALVES AND SANITARY SEWER MANHOLES. THE LETTERS "W" AND "S" ARE TO BE LOCATED DIRECTLY OVER THE HOUSE SERVICE LINES.
- 14. SEDIMENT AND EROSION CONTROL: THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR PROVIDING NECESSARY AND ADEQUATE MEASURES FOR PROPER CONTROL OF EROSION AND SEDIMENT RUNOFF FROM THE SITE ALONG WITH PROPER MAINTENANCE AND INSPECTION IN COMPLIANCE WITH THE NPDES GENERAL PERMIT FOR STORM WATER DISCHARGE ASSOCIATED WITH CONSTRUCTION ACTIVITY. ALL SEDIMENT AND EROSION CONTROL MEASURES REQUIRED IN THE PLANS, THE DISTRICT CONSTRUCTION AND MATERIAL SPECIFICATIONS, AND THE OHIO EPA STORM WATER REGULATIONS AND GENERAL PERMIT SHALL BE IN PLACE AND OPERATIONAL PRIOR TO CONSTRUCTION BEGINNING IN THE WORK AREA
- 15. PROHIBITED CONSTRUCTION ACTIVITIES: THE FOLLOWING CONSTRUCTION ACTIVITIES ARE PROHIBITED ON THE PROJECT.
- USING ANY SUBSTANCE OTHER THAN WATER TO CONTROL DUST.
- TRACKING OF MUD, DIRT AND DEBRIS ONTO ANY PUBLIC ROADWAY.
- OPEN BURNING OF PROJECT DEBRIS WITHOUT A PERMIT. THE CONTRACTOR IS RESPONSIBLE FOR OBTAINING THE PERMIT OR DISPOSING OF THE TREES AND STUMPS.
- PUMPING OF SEDIMENT-LADEN WATER FROM TRENCHES OR OTHER EXCAVATIONS INTO ANY SURFACE WATERS, ANY STREAM CORRIDORS, ANY WETLANDS, OR STORM SEWERS.
- DISCHARGING POLLUTANTS SUCH AS CHEMICALS, FUELS, LUBRICANTS, BITUMINOUS MATERIALS, RAW SEWAGE — AND OTHER HARMFUL WASTE INTO OR ALONGSIDE RIVERS, STREAMS, AND IMPOUNDMENTS OR INTO NATURAL OR MAN—MADE CHANNELS LEADING THERETO.
- STORING CONSTRUCTION EQUIPMENT AND VEHICLES AND/OR STOCKPILING CONSTRUCTION MATERIALS ON PROPERTY, PUBLIC OR PRIVATE, NOT PREVIOUSLY SPECIFIED FOR SAID PURPOSES.
- DISPOSING OF EXCESS OR UNSUITABLE EXCAVATED MATERIAL IN WETLANDS OR FLOODPLAINS, EVEN WITH THE PERMISSION OF THE PROPERTY OWNER.
- INDISCRIMINATE, ARBITRARY, OR CAPRICIOUS OPERATION OF EQUIPMENT IN ANY STREAM CORRIDORS, WETLANDS, SURFACE WATERS, OR OUTSIDE THE EASEMENT AREA
- PERMANENT OR UNSPECIFIED ALTERATION OF THE FLOW LINE OF A STREAM.
- REMOVAL OF TREES AND BUSHES, OR DAMAGING VEGETATION OUTSIDE THE LIMITS OF THE CONSTRUCTION AREA.
- DISPOSAL OF TREES, BRUSH AND OTHER DEBRIS IN ANY STREAM CORRIDORS, ANY WETLANDS, ANY SURFACE WATERS OR AT UNSPECIFIED LOCATIONS.
- 16. AS-BUILTS: ALL BENDS, FITTINGS, MAINLINE VALVES, WATER SERVICE VALVES, FIRE HYDRANTS, MANHOLES, AND SANITARY SEWER SERVICES MUST BE GPS LOCATED AND SHOWN ON THE AS-BUILT DRAWINGS USING THE OHIO STATE PLANE COORDINATE

SYSTEM. COORDINATE TABLE SHALL BE PROVIDED IN EXCEL FORMAT. TWO (2) FULL SIZE (2'X3') AND TWO (2) HALF SIZE (1'X1.5') SETS OF AS-BUILT CONSTRUCTION DRAWINGS IN AUTOCAD AND PDF FORMAT SHALL BE PROVIDED TO THE LICKING REGIONAL WATER DISTRICT AS SOON AS ALL PUBLIC IMPROVEMENTS ARE COMPLETED AND PRIOR TO FINAL ACCEPTANCE. ALL DOCUMENTS SHALL BE UPLOADED INTO A SECURE, CLOUD-BASED FILE STORAGE SITE.

17. SPECIFICATION DEVIATIONS: THE LICKING REGIONAL WATER DISTRICT RESERVES THE RIGHT TO DEVIATE FROM ITS' CONSTRUCTION MATERIAL SPECIFICATIONS ON A CASE—BY—CASE BASIS AT THE SOLE DISCRETION OF THE DISTRICT ADMINISTRATION. DEVIATIONS SHALL BE IN THE BEST INTEREST OF THE DISTRICTS' CUSTOMERS AND/OR PROVIDE A BETTER MEANS OF OPERATIONS AND MAINTENANCE.

DISTRICT WATERLINE NOTES

- 1. CONNECTING WATERLINES: THE CONNECTION OF PROPOSED WATERLINES TO EXISTING WATERLINES SHALL BE DONE IN A MANNER THAT WILL CAUSE A MINIMUM OF INCONVENIENCE TO THOSE WITH AFFECTED SERVICES.WORK CONCERNING THE DISCONNECTION AND RE-CONNECTION OF EXISTING WATERLINES SHALL BE DONE BETWEEN THE HOURS OF 10:00 P.M. AND 5:00 A.M., OR AS DIRECTED BY THE DISTRICT. NO SUCH WORK SHALL BEGIN UNTILTHE FIRE DEPARTMENT, DISTRICT, COUNTY SHERIFF'S OFFICE AND RESIDENTS WHOSE SERVICES WILL BE AFFECTED ARE ALL NOTIFIED AT LEAST ONE HUNDRED TWENTY (120) HOURS PRIOR TO THE CONNECTION, OF THE EXTENT, NATURE AND TIME OF THE ANTICIPATED WORK, NOR UNTIL THE METHOD AND SCHEDULE OF SUCH WORK HAS BEEN APPROVED BY THE DISTRICT.
- 2. SERVICE LOCATIONS: ALL WATER SERVICES SHALL BE LAID AT LEAST 10 FEET HORIZONTALLY FROM THE SANITARY SEWER SERVICE AND IN A SEPARATE TRENCH. A PERMIT FOR EACH WATER SERVICE MUST BE OBTAINED FROM THE DISTRICT, PRIOR TO MAKING ANY CONNECTION FROM THE WATER MAIN OR WATER SERVICE BOX TO ANY EXISTING OR PROPOSED BUILDING.
- 3. CONFLICTS: WHEN CONFLICTS IN GRADE BETWEEN WATERLINES AND SEWERS ARE FOUND DURING CONSTRUCTION, THE WATERLINES SHALL BE LOWERED, UNLESS DIRECTED OTHERWISE BY THE DISTRICT. A MINIMUM VERTICALSEPARATION OF 18 INCHES, MEASURED FROM THE OUTSIDE OF EACH PIPE, SHALL BE MAINTAINED.
- 4. MINIMUM DEPTH: WATER LINES SHALL BE LAID AS SHOWN ON DRAWINGS AND WITH A MINIMUM OF FOUR (4) FEET OF COVER FROM THE FINAL PROPOSED GROUND OR PAVEMENT GRADE TO THE TOP OF THE WATERLINE.
- 5. LINE CROSSINGS: AT ALL POINTS OF CROSSING OF WATER MAINS AND SEWERS, THE BACKFILL SHALL BE GRANULAR MATERIAL BETWEEN THE DEEPER AND SHALLOWER PIPE. THE MINIMUM HORIZONTAL SEPARATION BETWEEN WATER MAINS AND ALL SEWERS SHALL BE TEN (10) FEET MEASURED FROM THE OUTSIDE OF EACH PIPE. THE MINIMUM VERTICAL SEPARATION AT CROSSINGS OF WATER MAINS AND ALL SEWERS SHALL BE 18 INCHES MEASURED FROM THE OUTSIDE OF EACH PIPE.
- 6. DISINFECTION: ALL WATER MAINS SHALL BE CLEANED AND DISINFECTED IN ACCORDANCE WITH THE APPLICABLE SECTIONS OF AWWA SPECIFICATION C651. SPECIAL ATTENTION IS DIRECTED TO THE REQUIREMENTS OF FLUSHING AND CHLORINATING VALVES AND FIRE HYDRANTS. RESULTS OF THE DISINFECTION TESTS SHALL BE FURNISHED TO THE DISTRICT PRIOR TO ACCEPTANCE OF THE SYSTEM. TESTING FOR ACCEPTANCE TO BE CONDUCTED AFTER ALL OTHER UTILITIES LOCATED WITHIN THE RIGHT—OF—WAY ARE INSTALLED.
- 7. TESTING: A HYDROSTATIC TEST, AS REQUIRED IN SECTION 7.3 OF AWWA SPECIFICATION C605 FOR PVC PIPE OR SECTION 5.2 OF AWWA SPECIFICATION C600 FOR DUCTILE IRON PIPE AS APPLICABLE, SHALL BE APPLIED TO THE WATER MAIN. IF THERE ARE INDICATIONS OF LEAKS UNDER THIS PRESSURE TEST, THE CONTRACTOR SHALL LOCATE AND REPAIR ALL LEAKS AT THE CONTRACTOR'S EXPENSE UNTIL THE LEAKAGE IS WITHIN THE SPECIFIED ALLOWANCE. ALL BENDS, JOINT DEFLECTIONS AND HYDRANTS SHALL HAVE CONCRETE BACKING, AND ALL VALVES SHALL HAVE CONCRETE SUPPORTS, IN ACCORDANCE WITH THE STANDARD CONSTRUCTION DRAWINGS. TESTING FOR ACCEPTANCE TO BE CONDUCTED AFTER ALL OTHER UTILITIES LOCATED WITHIN THE RIGHT—OF—WAY ARE INSTALLED.
- 8. FIRE HYDRANTS: FIRE HYDRANTS SHALL BE AMERICAN FLOW CONTROL MODEL MK-73-5, MUELLER SUPER CENTURION 250 MODEL A-421 OR CLOW MEDALLION, AS SHOWN ON STANDARD DRAWING W-20, AND BE INSTALLED AS PER STANDARD DRAWINGS W-21, W-22, W-23, W-24 AND W-25. WEST LICKING FIRE DEPARTMENT REQUIRES ALL FIRE HYDRANTS TO HAVE A "SCREW ON" TYPE STORTZ FITTING. FIRE HYDRANTS SHALL BE PAINTED FIRE PROTECTION RED FROM THE MANUFACTURER AND THE LIDS OF WATCH VALVE BOXES PAINTED FIRE PROTECTION RED IN THE FIELD. FIRE HYDRANTS ON A PRIVATE WATER SYSTEM SHALL BE PAINTED FIRE PROTECTION RED WITH A BLUE BONNET. THE DISTRICT MAY REQUIRE SPECIFIC HYDRANTS TO MATCH EXISTING DEVELOPMENTS.
- 9. CURB AND VALVE BOXES: CURB BOXES SHALL BE LOCATED 6 INCHES FROM THE FRONT PROPERTY LINE OR EASEMENT LINE, AND WITHIN 10 FEET OF THE SIDE PROPERTY LINE, UNLESS OTHERWISE SHOWN ON THE PLANS. ALL CURB BOX AND VALVE BOX TOPS SHALL BE ADJUSTED TO BE 3" ABOVE FINAL SURFACE GRADES. THE CONTRACTOR SHALL FURNISH AND PLACE, AS DIRECTED, A STAKE MADE OF 4" X 4" HARDWOOD LUMBER AT ALL CURB BOXES AND VALVE BOXES, EXTENDING A MINIMUM OF 3 FEET ABOVE FINAL SURFACE GRADES WITH THE TOP 2 FEET OF THE 4"X4" BEING PAINTED SAFETY BLUE. ALL CURB AND VALVE BOX LIDS SHALL BE PAINTED SAFETY BLUE.
- 10. CURB BOX EXTENSION ROD: A FORD ROD—42 EXTENSION ROD WITH CENTERING RING SHALL BE INSTALLED ON ALL CURB BOXES.
- 11. VALVE EXTENSION: IF THE TOP OF THE OPERATING NUT IS LOWER THAN 36 INCHES BELOW FINISHED GRADE, AN EXTENSION STEM SHALL BE FURNISHED TO BRING THE TOP OF THE OPERATING NUT TO BETWEEN 24 INCHES AND 36 INCHES OF FINISHED GRADE ELEVATION.



E ROUTE 161 - MINK STREET WATERLINE EXTEN: LICKING REGIONAL WATER DISTRICT

AND

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JOB NO. 4379.006

PROJECT MGR.



SHEET

DISTRICT WATERLINE NOTES (CONT.)

- 12. INSTALLATION IN EMBANKMENT: WHERE WATER MAINS ARE TO BE INSTALLED IN EMBANKMENT AREAS, THE EMBANKMENT SHALL BE PLACED AND COMPACTED IN ACCORDANCE WITH THE SPECIFICATIONS PRIOR TO THE INSTALLATION OF THE WATER MAIN. THE WATER MAIN SHALL BE INSTALLED WITH A MINIMUM OF FOUR (4) FEET OF COVER IN ALL DIRECTIONS.
- 13. VALVE OPERATION: EXISTING VALVES SHALL BE OPERATED BY DISTRICT PERSONNEL ONLY.
- 14. CONSTRUCTION AND MATERIAL SPECIFICATIONS: ALL MATERIALS AND CONSTRUCTION SHALL MEET THE REQUIREMENTS OF THE CURRENT LICKING REGIONAL WATER DISTRICT CONSTRUCTION AND MATERIAL SPECIFICATIONS, INCLUDING ALL SUPPLEMENTS THERETO (UNLESS TOWNSHIP, CITY, AND/OR COUNTY STANDARDS ARE MORE STRINGENT, IN WHICH CASE THOSE STANDARDS SHALL BE FOLLOWED). WATER MAIN PIPE SHALL BE PVC PLASTIC PIPE, AWWA C900 DR 18. DUCTILE IRON PIPE CLASS 52, AWWA C151, CEMENT LINED AWWA C104, WITH JOINTS CONFORMING TO AWWA C111 IS AN ACCEPTABLE ALTERNATE WATER MAIN PIPE. ALL BENDS, JOINT DEFLECTIONS AND FITTINGS SHALL BE BACKED WITH CONCRETE. BLUE METALLIC FIELD LOCATOR TAPE OF SIX (6) INCH WIDTH SHALL BE PLACED OVER ALL WATER MAINS, WITHIN 12 TO 18 INCHES OF FINISHED GRADE. TWELVE (12) GAUGE EXTRA HIGH STRENGTH TRACER WIRE SHALL BE LAID IN THE PIPE TRENCH IN ACCORDANCE WITH SWLCWS DISTRICT CMS SECTION 212.01(C). TRACER WIRE CONNECTIONS SHALL BE MADE WITH COPPERHEAD SNAKEBITE WATERPROOF DIRECT BURY LUGS #3WB-01 AND PROVIDED IN ACCORDANCE WITH LICKING REGIONAL WATER DISTRICT CMS SECTION 210.06.
 - THE MINIMUM LENGTH OF PIPE TO BE RESTRAINED SHALL BE AS SHOWN IN THE FOLLOWING TABLE:

REQUIRED LENGTH OF RESTRAINED PIPE BEYOND FITTING IN FEET:

FITTING (PIPE DIAMETER)	MINIMUM LENGTH (FT)
45 DEGREE BEND (≤ 8 INCHES)	18
45 DEGREE BEND (10 INCH TO 16 INCHES)	36
22 1/2 DEGREE BEND (≤ 16 INCHES)	18
11 1/4 DEGREE BEND (≤ 16 INCHES)	9
FIRE HYDRANT LEADS	ALL JOINTS
END OF LINE TEES (6 INCHES TO 8 INCHES)	36 (ALONG BRANCH)
END OF LINE TEES (14 INCHES TO 16 INCHES)	72 (ALONG BRANCH)

*RESTRAINED RUN LENGTH ON TEES ASSUMED 18 FEET ON EACH SIDE OF FITTING.

WATER MAIN VALVES SHALL BE AWWA C509, RESILIENT WEDGE WITH 250 PSI WORKING PRESSURE, NONRISING STEM, LEFT HAND OPEN VALVE WITH RUBBER "O" PACKING SEALS. ALL VALVE BONNET BOLTS SHALL BE STAINLESS STEEL.

WATER SERVICE LINE PIPE SHALL BE AWWA C901, PE 4710, DR9, CTS ASTM D2737 AND SHALL BE INSTALLED WITH A COVER OF FOUR (4) FEET.

- 15. WATER SYSTEM PRESSURE: ALL WATER MAINS INCLUDING THOSE NOT DESIGNED TO PROVIDE FIRE PROTECTION, SHALL BE SIZED AFTER A HYDRAULIC ANALYSIS BASED ON FLOW DEMANDS AND PRESSURE REQUIREMENTS. THE SYSTEM SHALL BE DESIGNED TO MAINTAIN A MINIMUM PRESSURE OF 20 PSI (140 KPA) AT GROUND LEVEL AT ALL POINTS IN THE DISTRIBUTION SYSTEM UNDER ALL CONDITIONS OF FLOW. THE NORMAL WORKING PRESSURE IN THE DISTRIBUTION SYSTEM SHALL BE AT LEAST 35 PSI (240 KPA) AND SHOULD BE APPROXIMATELY 60 TO 80 PSI (410 550 KPA) AND NOT LESS THAN 35 PSI (240 KPA).
- 16. CURB STOP: CURB STOP SHALL BE EQUIVALENT TO MUELLER H-15209 WITH A BOX EQUIVALENT TO BINGHAM AND TAYLOR NO. 4901, SIZE 94E. ALL CURB STOPS SHALL BE PROVIDED WITH VADLE CURB BOX LOCK.
- 17. CORPORATION STOP AND SADDLE: CORPORATION STOPS SHALL BE MUELLER H-15008 WITH A FORD STYLE "FS" SERIES 313 TAPPING SADDLE OR DISTRICT PRE-APPROVED EQUAL.
- 18. TAPPING SLEEVE: TAPPING SLEEVES SHALL BE STAINLESS STEEL AND SHALL BE EQUIVALENT TO MUELLER H-304 OR SMITH BLAIR 663.
- 19. BACKFLOW PREVENTERS: BACKFLOW PREVENTERS SHALL BE PROVIDED ON ALL COMMERCIAL CONNECTIONS AND ANY RESIDENTIAL CONNECTIONS WHERE AN AUXILIARY WATER SUPPLY IS AVAILABLE. ALL BACKFLOW PREVENTERS SHALL HAVE AN APPROVED EXPANSION TANK.

REDUCED PRESSURE BACKFLOW PREVENTERS SHALL BE PROVIDED FOR ALL AREAS DEEMED BY THE DISTRICT WHERE THERE IS A HIGH POTENTIAL HEALTH HAZARD FROM CONTAMINATION. ALL REDUCED PRESSURE BACKFLOW PREVENTERS SHALL MEET AWWA C511 AND BE FROM THE LATEST APPROVED LIST OF THE OEPA.

DOUBLE CHECK BACKFLOW PREVENTERS SHALL BE PROVIDED FOR ALL AREAS DEEMED BY THE DISTRICT WHERE THERE IS A LOW POTENTIAL HEALTH HAZARD FROM CONTAMINATION. ALL DOUBLE CHECK BACKFLOW PREVENTERS SHALL MEET AWWA C510 AND BE FROM THE LATEST APPROVED LIST OF THE OEPA.

- 20. BOOSTER PUMPS: NO PERSON SHALL INSTALL OR MAINTAIN A WATER SERVICE CONNECTION WHERE A BOOSTER PUMP HAS BEEN INSTALLED, UNLESS AN APPROVED METHOD IS IN PLACE AND OPERATIONAL TO MAINTAIN SUCTION PRESSURE IN ACCORDANCE WITH OHIO ADMINISTRATIVE CODE RULE 3745-95-07.
- 21. TOOLS AND SPARE PARTS: THE FOLLOWING TOOLS AND SPARE PARTS SHALL BE DELIVERED TO THE DISTRICT PRIOR TO FINAL ACCEPTANCE OF THE PROJECT: ONE (1) MAINLINE WRENCH, ONE (1) FORTY EIGHT INCH CURB BOX WRENCH (MUELLER H-10356), ONE (1) COMPLETE CURB BOX WITH EXTENSION ROAD AND CENTERING RING, ONE (1) FIRE HYDRANT WRENCH, AND ONE (1) STRAIGHT STORTZ FITTING (HARRINGTON HPHA50-45NH/CAP).

- 22. ALL WATER MAIN PLUGS, CAPS, TEES, HYDRANTS, BENDS, AND OTHER FITTINGS FOR WATER MAINS SHALL BE PROVIDED WITH RESTRAINED JOINTS IN ADDITION TO REACTION BACKING REQUIRED BY STANDARD DRAWINGS. REACTION BACKING SHALL BE PLACED SO THAT PIPE AND PIPE RESTRAINT ARE ACCESSIBLE FOR REPAIR. THE BEARING AREA BETWEEN THE FITTINGS, PIPE RESTRAINT, AND THE CONCRETE SHALL BE COVERED WITH MINIMUM 8 MIL POLYETHYLENE FILM TO BREAK THE BOND BETWEEN FITTING, PIPE RESTRAINT, AND CONCRETE. MECHANICAL JOINT RESTRAINT SHALL BE PROVIDED BASED ON TABLE IN NOTE 14, THIS SHEET, AND PROVIDED IN ACCORDANCE WITH LICKING REGIONAL WATER DISTRICT CMS SECTION 210.05.
- 23. MAINTAINING MAIL SERVICE: THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING MAIL SERVICE IN THE CONSTRUCTION AREA. PRIOR TO DISTURBING ANY MAIL BOXES THE CONTRACTOR SHALL CONTACT THE POSTAL AUTHORITIES AND SHALL TEMPORARILY RELOCATE MAILBOXES IN ACCORDANCE WITH THE REQUIREMENTS THEREFORE. THE CONTRACTOR SHALL RESTORE MAILBOXES THEIR ORIGINAL CONDITION AND LOCATION. COST TO BE INCLUDED IN THE PRICE BID FOR THE VARIOUS WATER MAIN ITEMS.
- 24. CONTRACTOR SHALL BE RESPONSIBLE FOR THE DISPOSAL OF MATERIALS USED IN AND CREATED BY HORIZONTAL DIRECTIONAL DRILLING ACTIVITIES IN ACCORDANCE WITH APPLICABLE LAWS AND REGULATIONS AT NO ADDITIONAL COST TO THE DISTRICT.
- 25. CONTRACTOR SHALL BE RESPONSIBLE FOR THE DISPOSAL OF EXCESS EXCAVATED MATERIALS NOT USED AS OR SUITABLE FOR BACKFILL IN ACCORDANCE WITH APPLICABLE LAWS AND REGULATIONS AT NO ADDITIONAL COST TO THE DISTRICT.

EXISTING MINOR CONTOUR LINE

<u>ABBREVIATIONS</u>

APPROX — APPROXIMATE

CMP — CORRUGATED METAL PIPE

CP — CONTROL POINT

CPP — CORRUGATED PLASTIC PIPE

DIA. — DIAMETER

DR — DIMENSION RATIO

EX. — EXISTING

FH — FIRE HYDRANT

HDD — HORIZONTAL DIRECTIONAL DRILLING

HDPE — HIGH DENSITY POLYETHYLENE

LF — LINEAR FEET

MIN. — MINIMUM

PROP. — PROPOSED

PVC — POLYVINYL CHLORIDE

RCP — REINFORCED CONCRETE PIPE

RJIB — RESTRAINED JOINT INTEGRAL BELL
R/W — RIGHT OF WAY
SR — STATE ROUTE

RD - ROAD

ST - STREET
STA. - STATION
WM - WATER MAIN
WS - WATER SERVICE

WV - WATER (GATE) VALVE

DENUNINARNS							
POINT#	NORTHING	EASTING	ELEVATION	DESCRIPTION			
100	757814.226	1915881.386	1221.76	5/8" REBAR WITH YELLOW PLASTIC SURVEY CAP			
101	758823.562	1915944.552	1205.60	5/8" REBAR WITH YELLOW PLASTIC SURVEY CAP			
102	759402.686	1915962.416	1213.68	5/8" REBAR WITH YELLOW PLASTIC SURVEY CAP			
103	760012.099	1915961.926	1222.63	5/8" REBAR WITH YELLOW PLASTIC SURVEY CAP			
104	760686.356	1916047.227	1229.55	5/8" REBAR WITH YELLOW PLASTIC SURVEY CAP			
105	761604.093	1916064.427	1207.20	5/8" REBAR WITH YELLOW PLASTIC SURVEY CAP			
106	762576.883	1916157.632	1185.00	5/8" REBAR WITH YELLOW PLASTIC SURVEY CAP			
107	763404.929	1916182.216	1194.80	5/8" REBAR WITH YELLOW PLASTIC SURVEY CAP			
108	761866.334	1916110.236	1204.08	5/8" REBAR WITH YELLOW PLASTIC SURVEY CAP			
109	763952.24	1916227.378	1197.37	5/8" REBAR WITH YELLOW PLASTIC SURVEY CAP			
110	759465.198	1915529.611	1234.02	5/8" REBAR WITH YELLOW PLASTIC SURVEY CAP			
111	759493.625	1915067.592	1248.24	5/8" REBAR WITH YELLOW PLASTIC SURVEY CAP			
112	759519.317	1914178.445	1235.55	5/8" REBAR WITH YELLOW PLASTIC SURVEY CAP			
113	759570.588	1913736.601	1239.23	5/8" REBAR WITH YELLOW PLASTIC SURVEY CAP			
114	759616.507	1912774.189	1221.16	5/8" REBAR WITH YELLOW PLASTIC SURVEY CAP			
115	759626.099	1912106.823	1222.57	5/8" REBAR WITH YELLOW PLASTIC SURVEY CAP			
116	759723.014	1911139.542	1212.51	5/8" REBAR WITH YELLOW PLASTIC SURVEY CAP			
117	757782.624	1916876.694	1212.68	5/8" REBAR WITH YELLOW PLASTIC SURVEY CAP			
118	757745.694	1917861.74	1205.33	5/8" REBAR WITH YELLOW PLASTIC SURVEY CAP			
119	757872.366	1918643.919	1194.39	5/8" REBAR WITH YELLOW PLASTIC SURVEY CAP			
120	757926.748	1918904.604	1193.02	5/8" REBAR WITH YELLOW PLASTIC SURVEY CAP			
121	758044.444	1919698.415	1179.06	5/8" REBAR WITH YELLOW PLASTIC SURVEY CAP			
122	758087.609	1920386.549	1155.89	5/8" REBAR WITH YELLOW PLASTIC SURVEY CAP			
123	757470.423	1915909.949	1215.94	5/8" REBAR WITH YELLOW PLASTIC SURVEY CAP			
124	757116.952	1915860.316	1212.32	5/8" REBAR WITH YELLOW PLASTIC SURVEY CAP			

BENCHMARKS

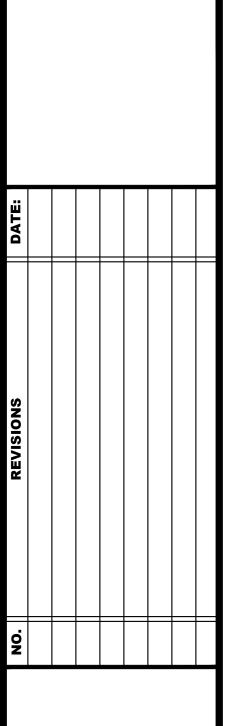
SURVEY NOTES:

LEGEND

- 1. THE TOPOGRAPHIC SURVEY WAS CONDUCTED USING GPS SURVEY TECHNIQUES PERFORMED BY STRAND ASSOCIATES, INC. BASED ON FIELD WORK CONDUCTED AT THE SITE IN FEBRUARY 2024.
- 2. THE BENCHMARKS SHOWN HEREON WERE ESTABLISHED AT THE PROJECT SITE USING REAL—TIME KINETIC GPS OBSERVATIONS. THE REAL—TIME KINETIC GPS OBSERVATIONS WERE MADE USING A TRIMBLE, MODEL R12, DUAL FREQUENCY RECEIVER. THE PROJECT AND REFERENCE MERIDIAN IS BASED ON THE OHIO SOUTH ZONE STATE PLANE COORDINATE SYSTEM (3402), NORTH AMERICAN DATUM OF 1983 (2011)
- 3. THE VERTICAL CONTROL AND ALL ELEVATIONS SHOWN HEREON CORRESPOND TO THE NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD88).

	<u>EXISTING</u>				
R/W	EXISTING RIGHT OF WAY	A	CONTROL POINT		EXISTING ROCK
<u>P</u>	EXISTING PROPERTY LINE	+	EXISTING BENCHMARK		EXISTING POST
G	EXISTING GAS LINE	₩ / *	EXISTING DECIDUOUS/CONIFEROUS TREE	[MB]	EXISTING MAILBOX
——————————————————————————————————————	EXISTING SANITARY SEWER		EXISTING TREE STUMP		EXISTING TELEPHONE PEDESTAL
	EXISTING WATER MAIN	(SEP) (TNK)	EXISTING SEPTIC TANK	A	ASHPALT DRIVE
ST	EXISTING STORM SEWER		EXISTING CATCH BASIN	©	CONCRETE DRIVE
	EXISTING CULVERT		EXISTING POWER POLE	G	GRAVEL DRIVE
т	EXISTING TELEPHONE LINE	\downarrow	EXISTING GUY WIRE	○I.P.F.	IRON PIN FOUND
——— Е ———	EXISTING UNDERGROUND ELECTRIC LINE		EXISITING LIGHT POLE	PRO	POSED
OH ———	EXISTING OVERHEAD ELECTRIC LINE		EXISTING SIGN	— W —	PROPOSED WATER MAIN
FO	EXISTING FIBER OPTIC LINE	НG	EXISTING GAS LINE MARKER	— W —	PROPOSED WATER MAIN W/ CASING
x x	EXISTING FENCE		EXISTING GAS METER	3	PROPOSED END CAP
	EXISTING ROAD CENTERLINE	\otimes WV	EXISTING WATER VALVE	⊗	PROPOSED WATER VALVE
	EXISITING DITCH CENTERLINE	\forall	EXISTING FIRE HYDRANT	8	PROPOSED FIRE HYDRANT
	EXISTING DRIVE	(WELL)	EXISTING WATER WELL		PROPOSED GRAVEL DRIVE REPAIR
	EXISTING MAJOR CONTOUR LINE		EXISTING MANHOLE		PROPOSED PAVEMENT REPAIR

EXISTING AIR CONDITIONING UNIT



IENERAL NOIES AND LEGEND

JOB NO. 4379.006

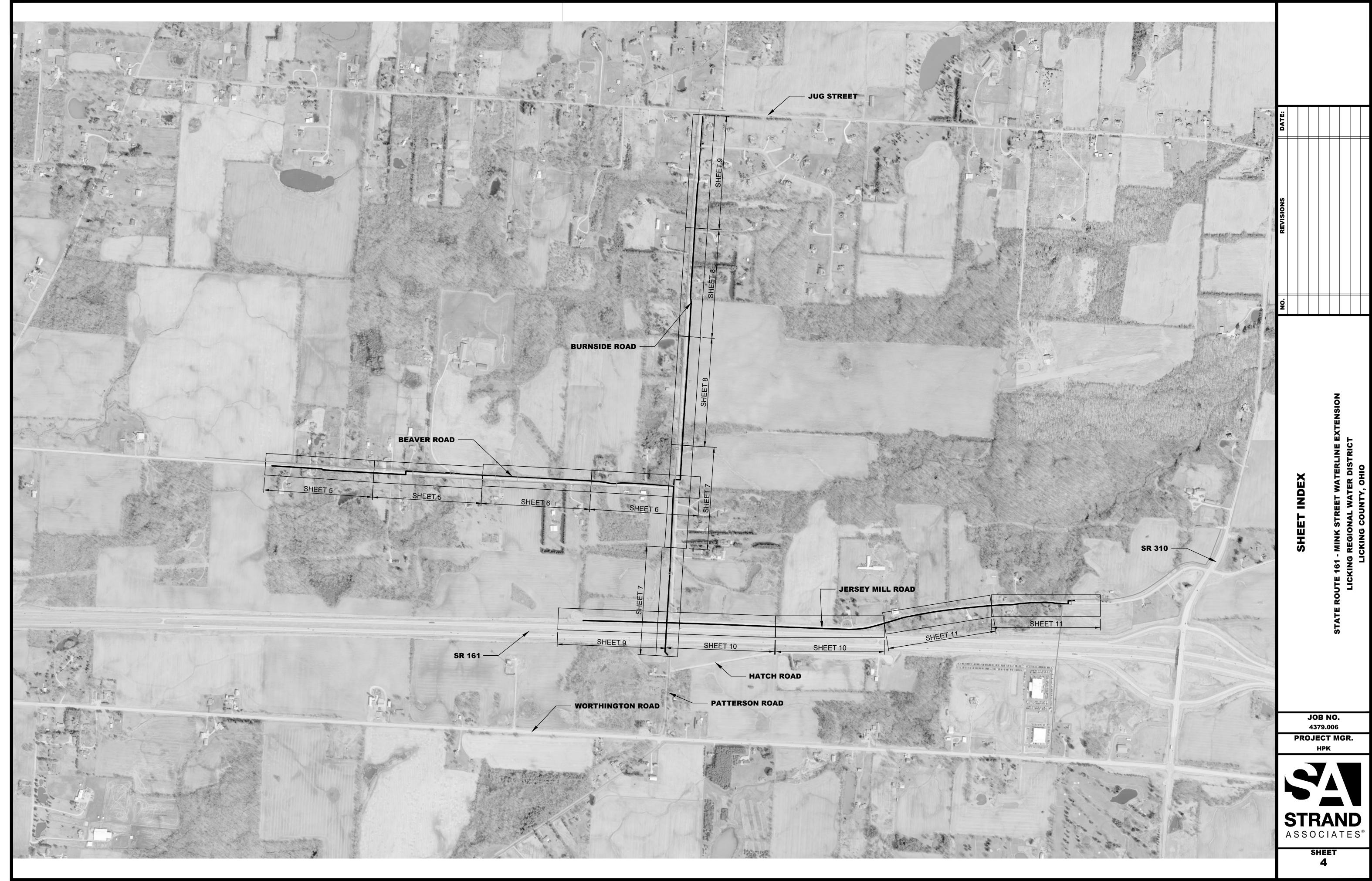
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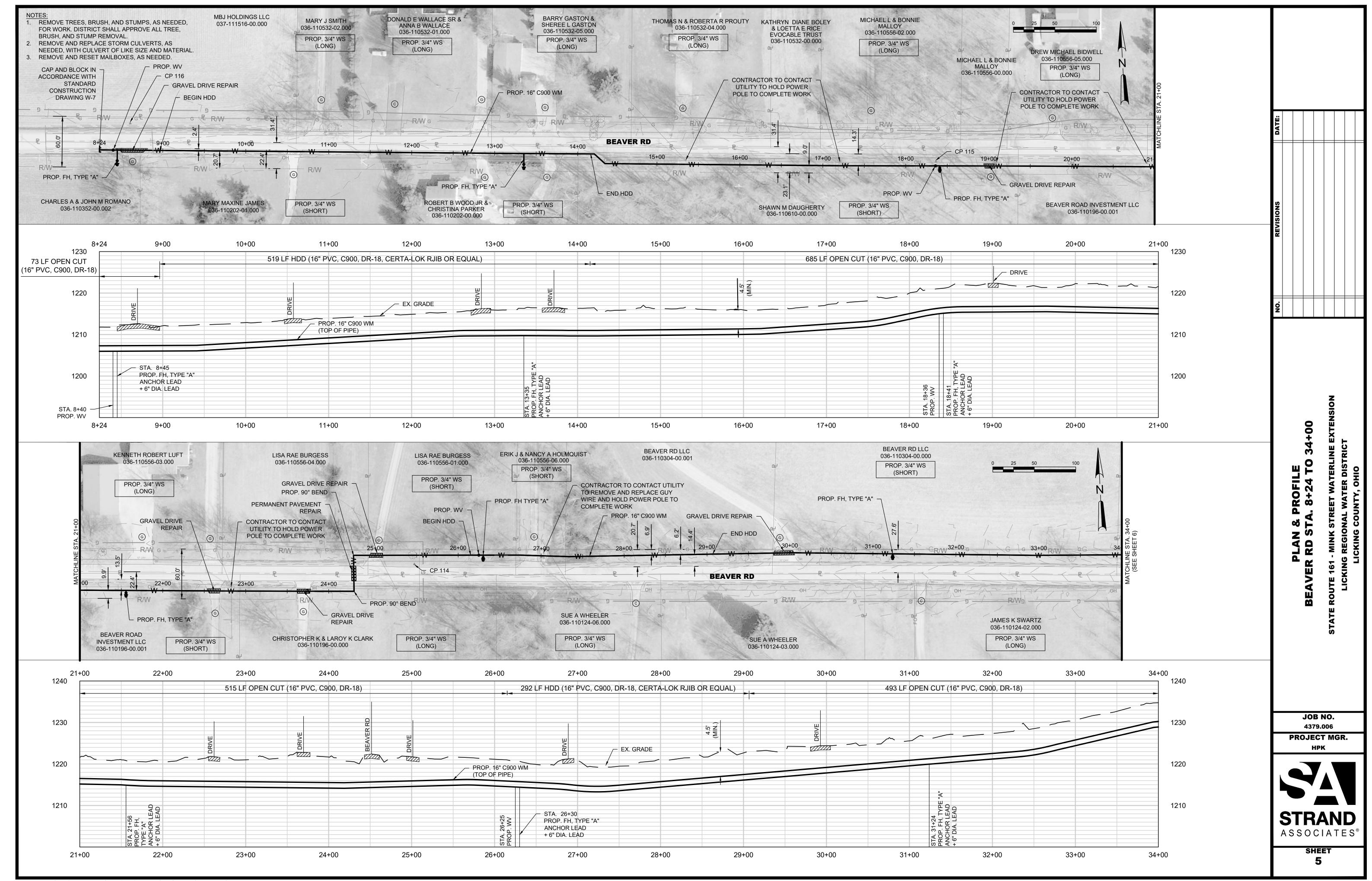
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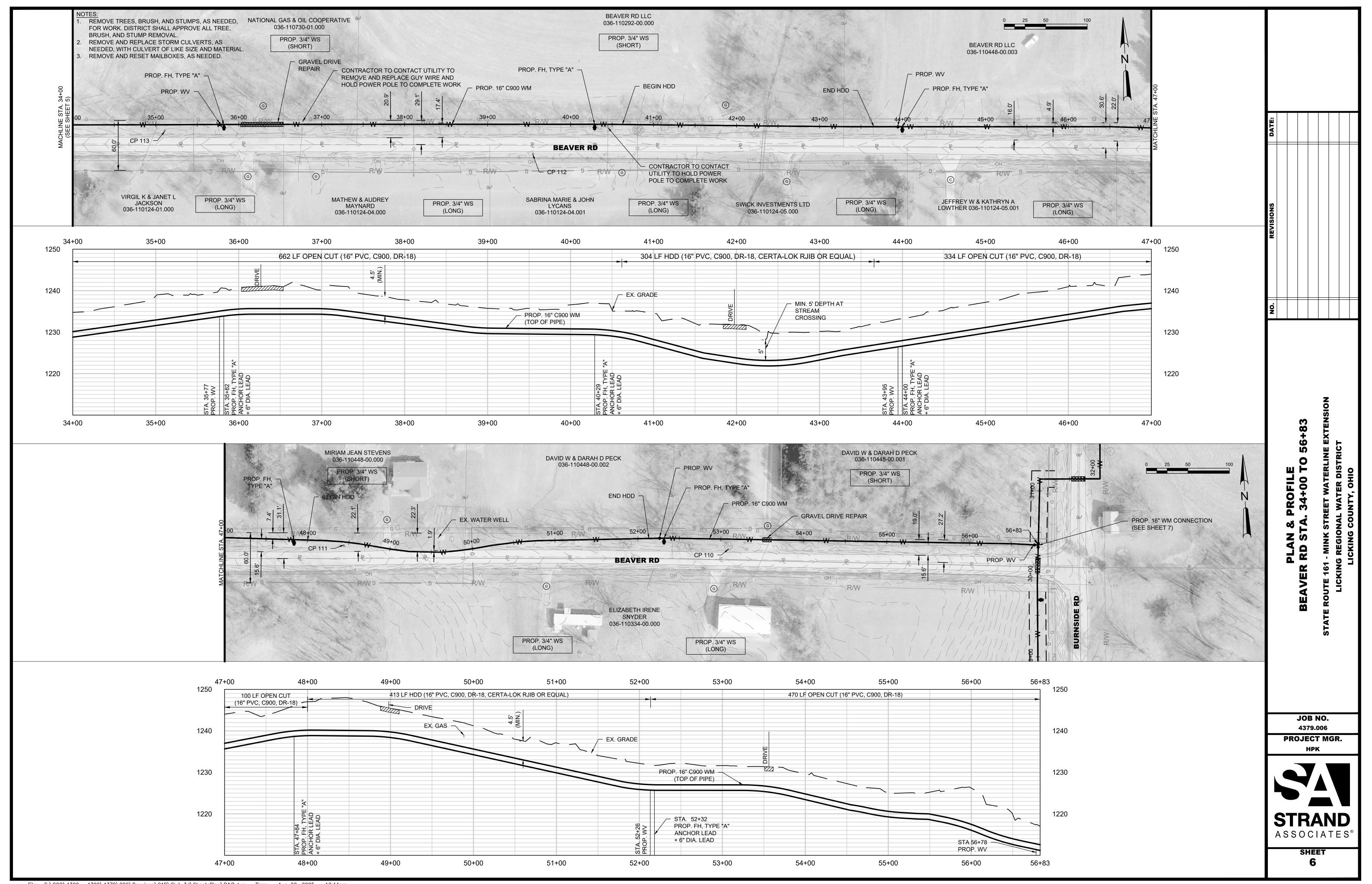
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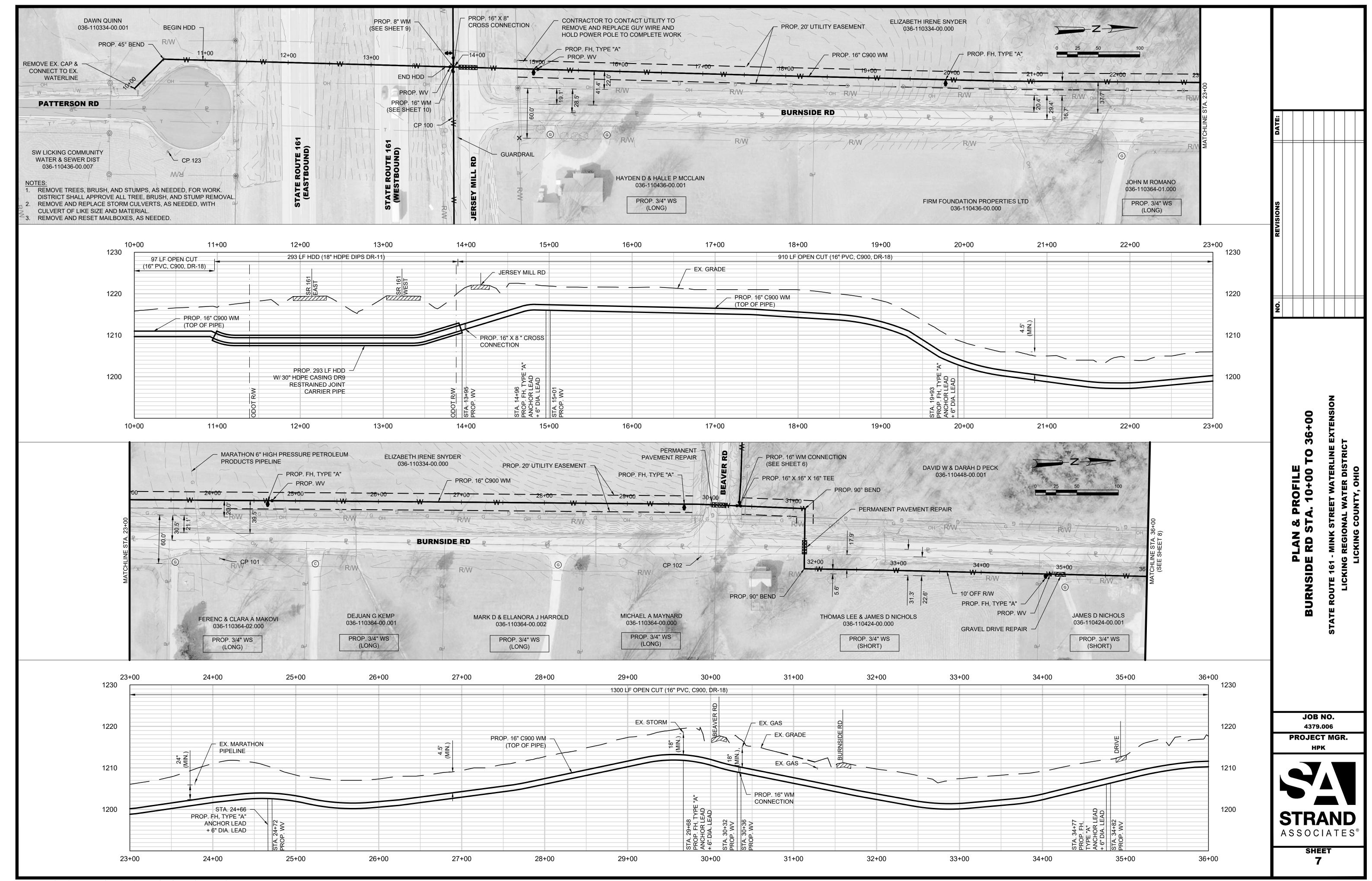


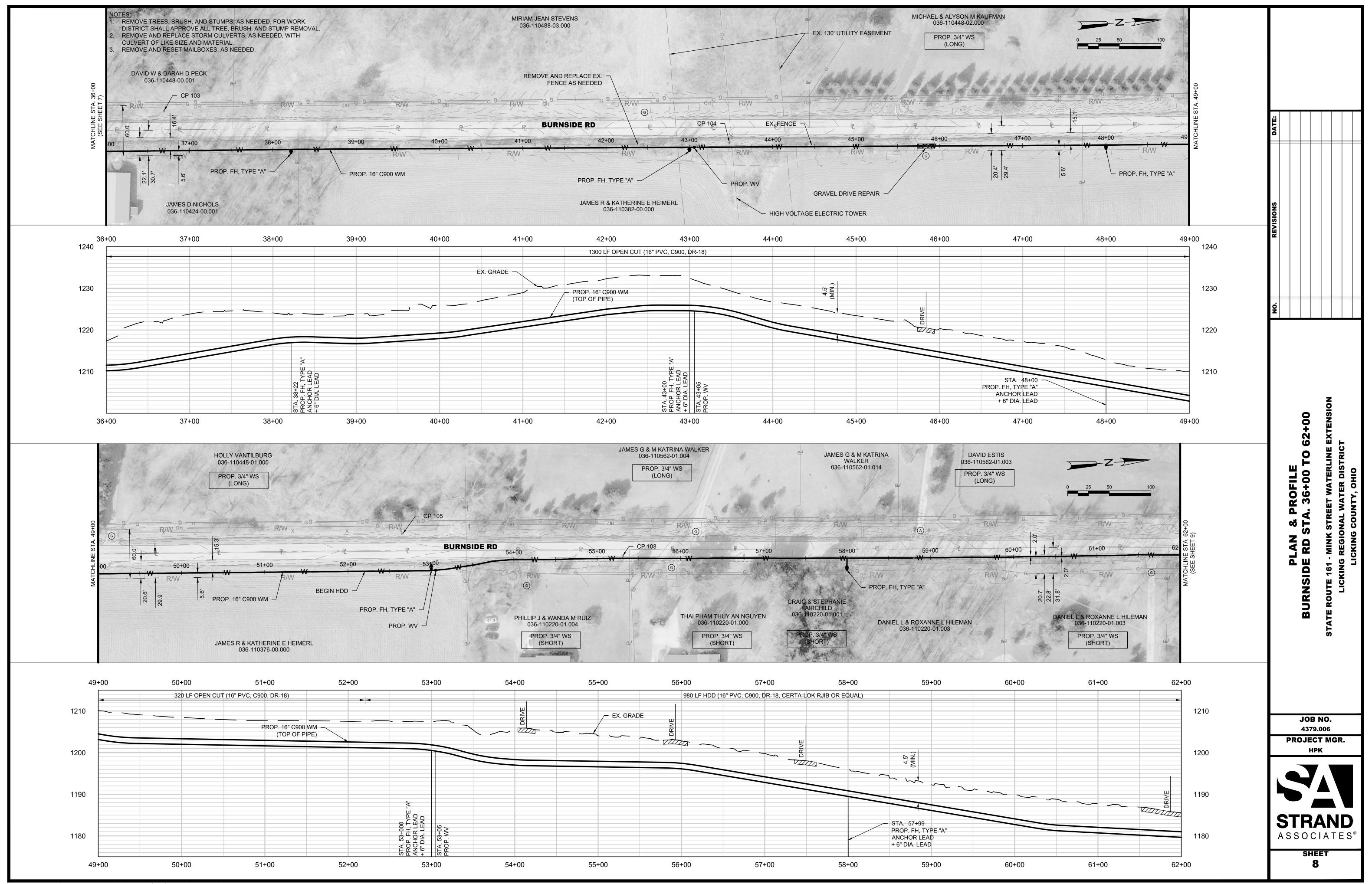
SHEET 3

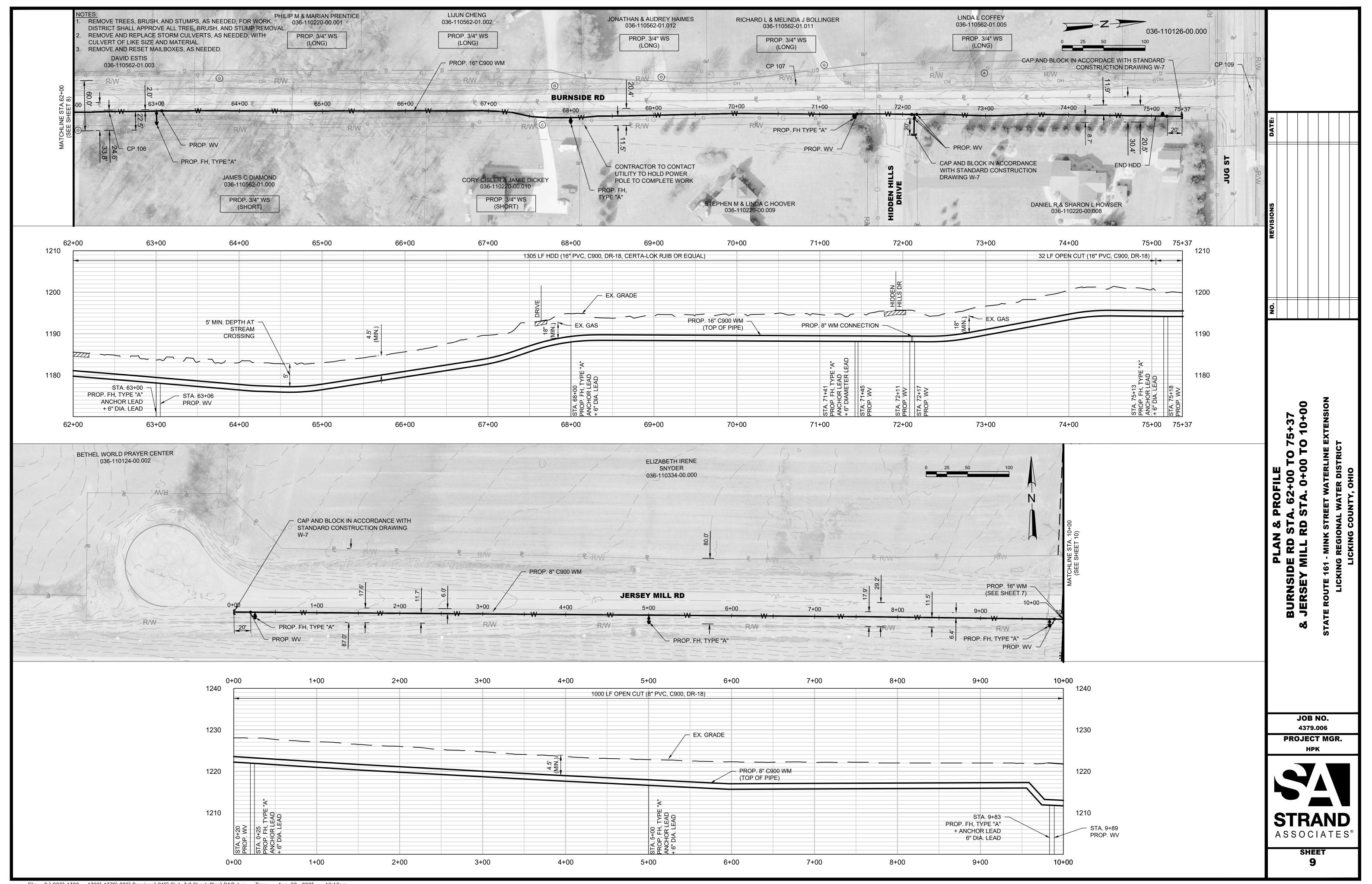


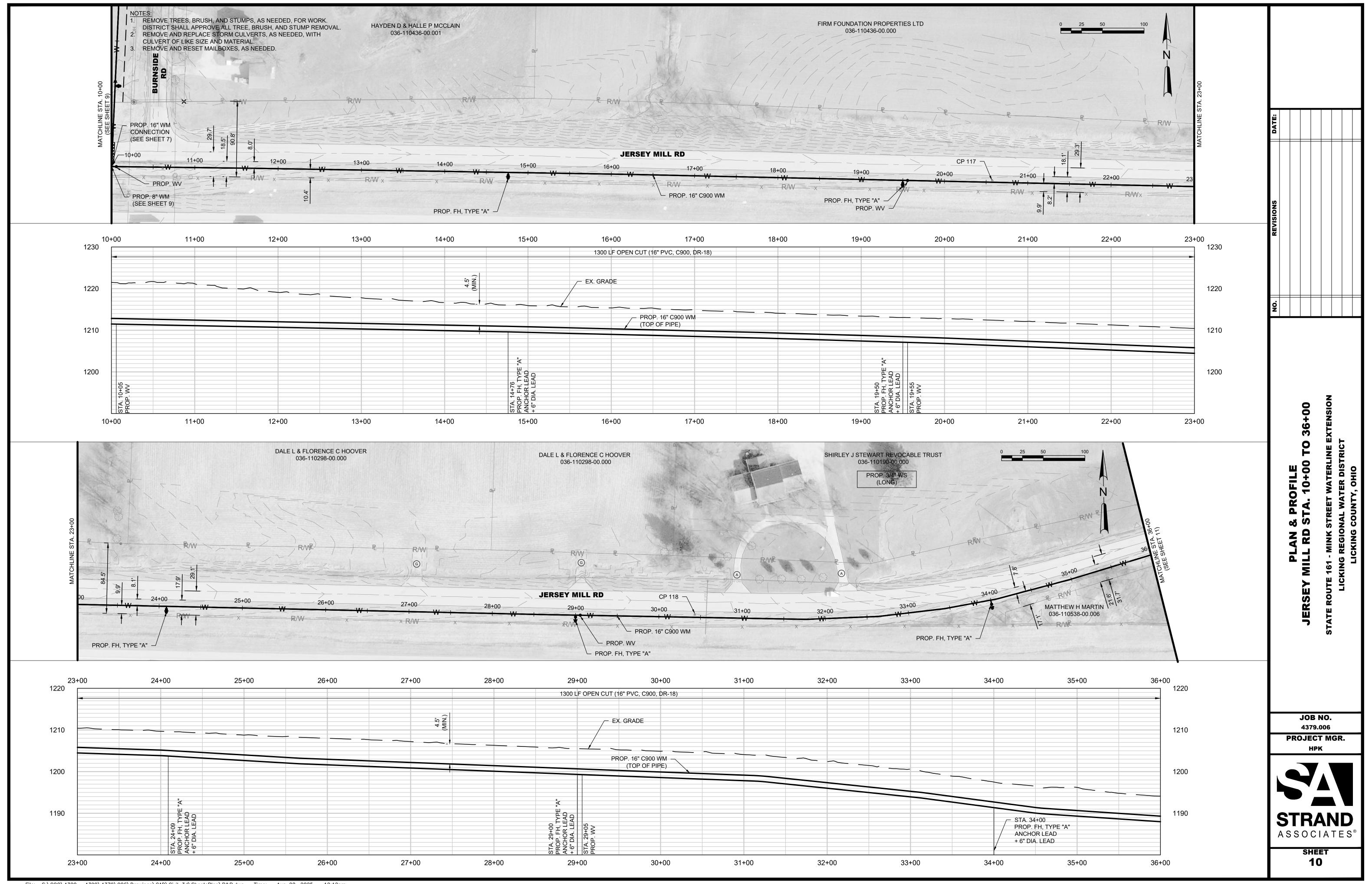


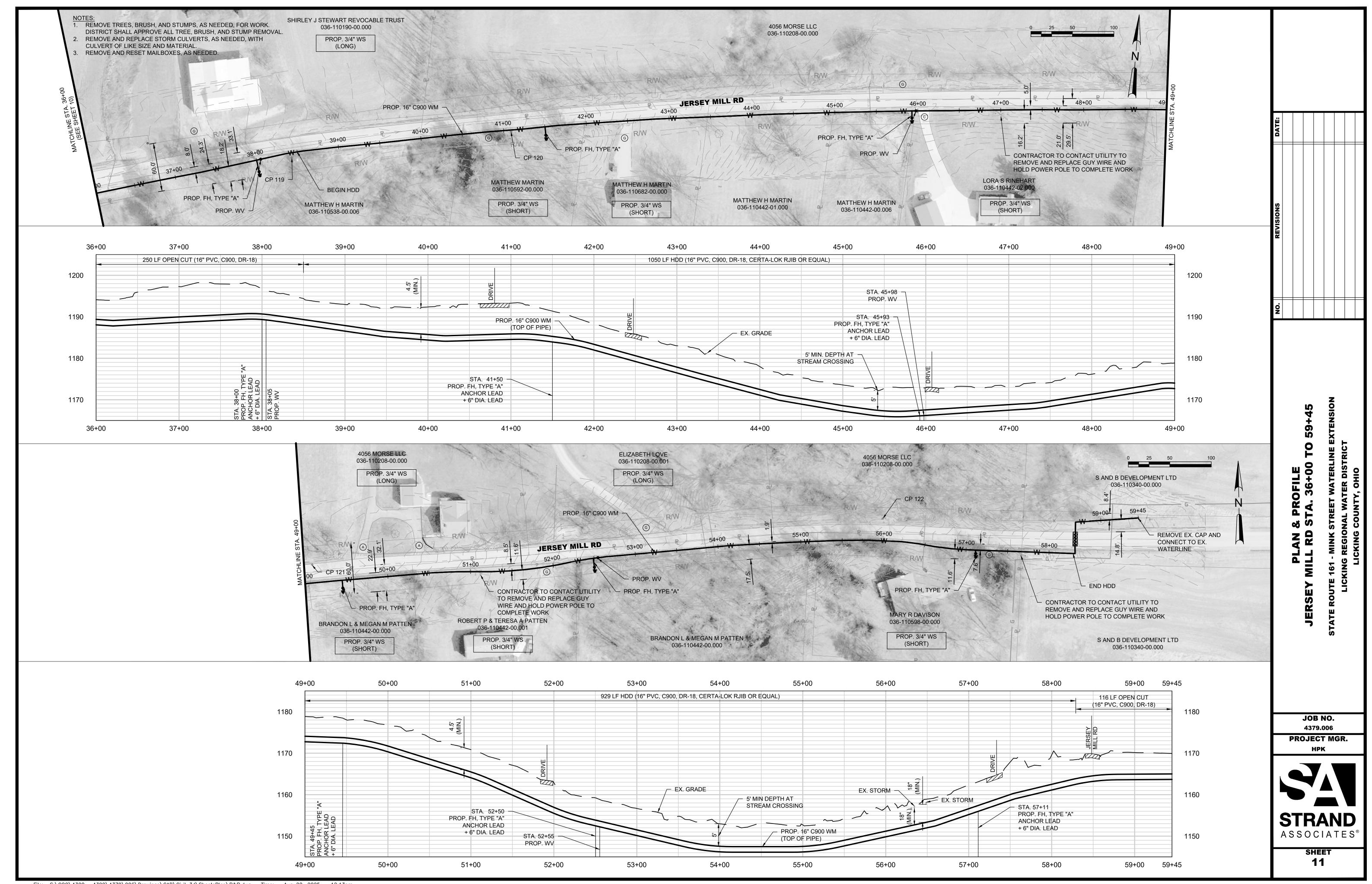


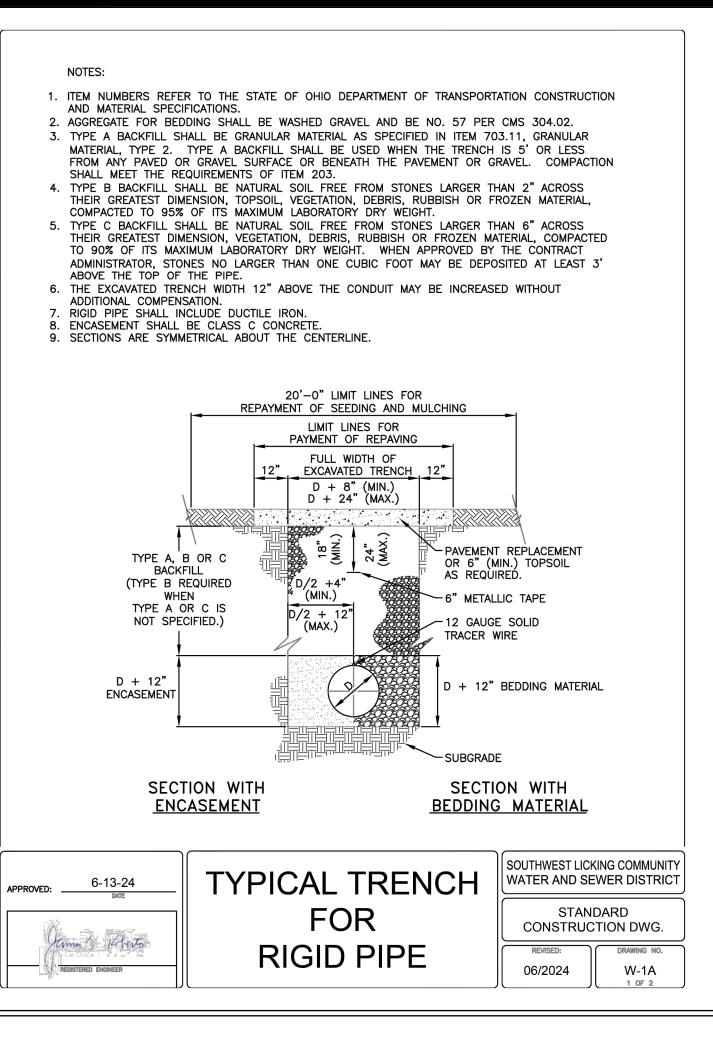


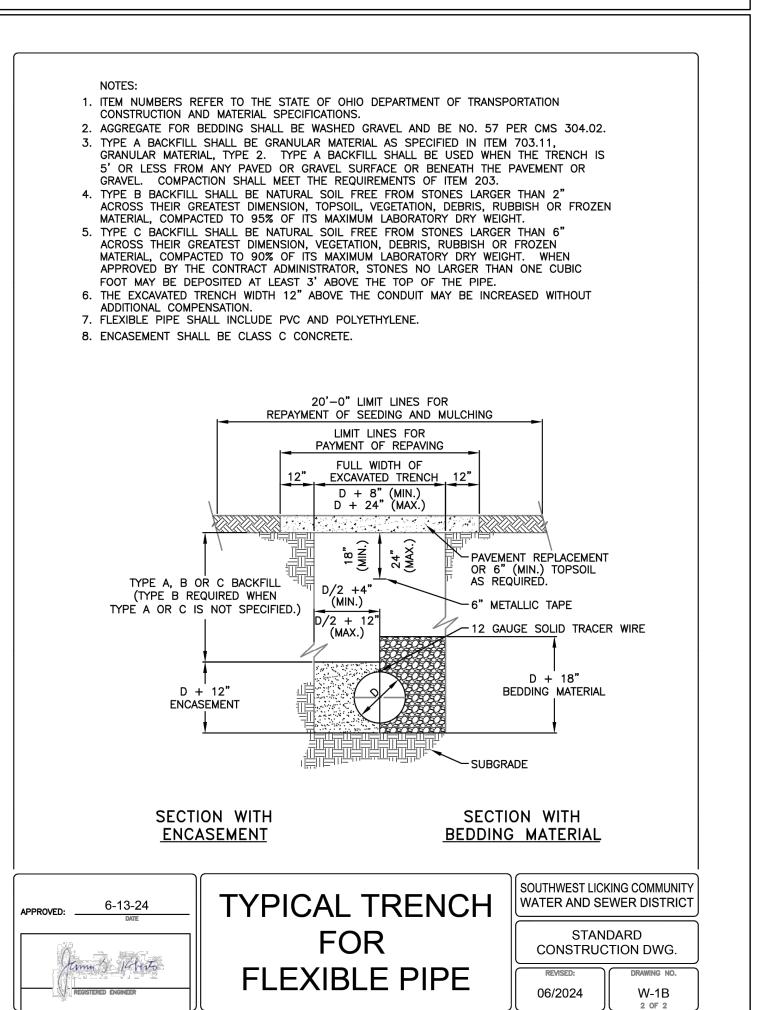


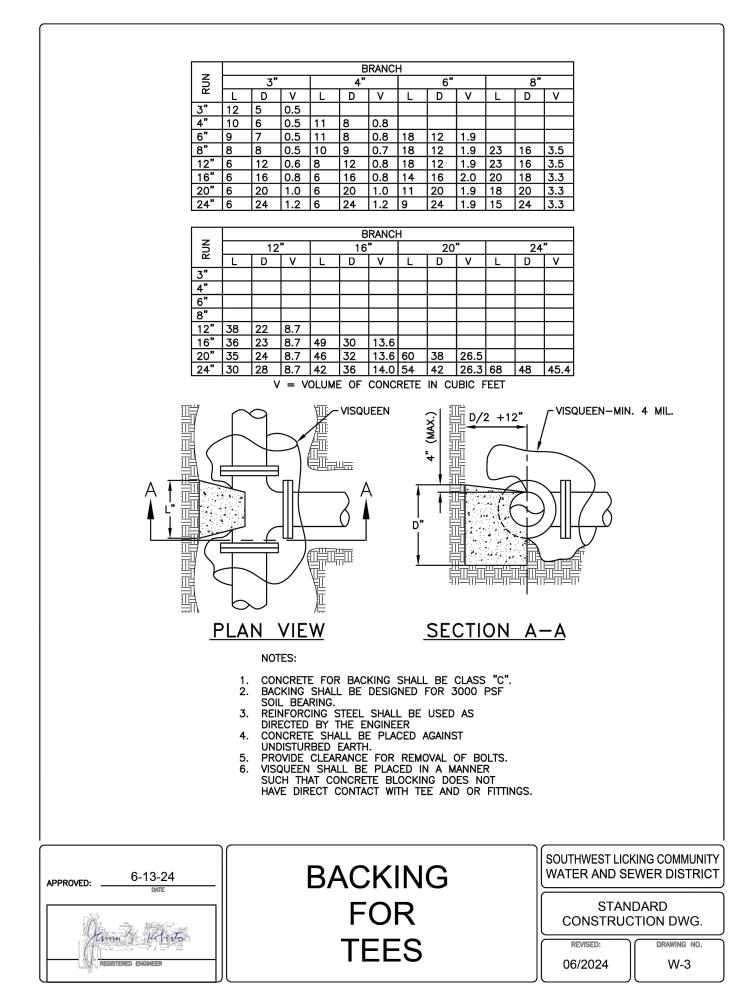


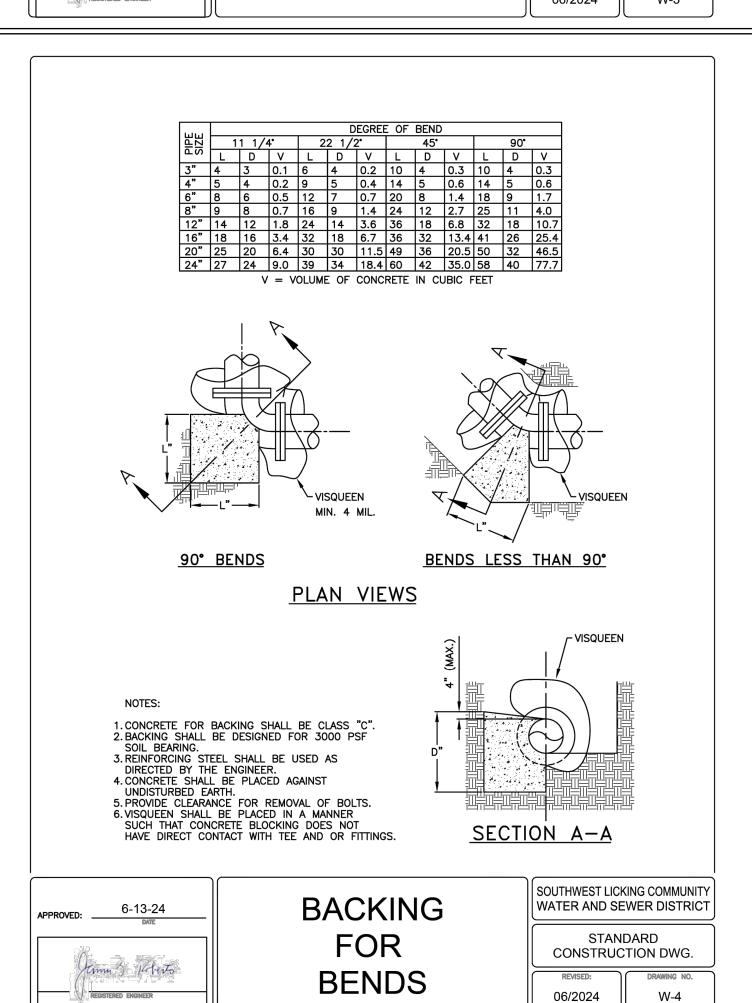


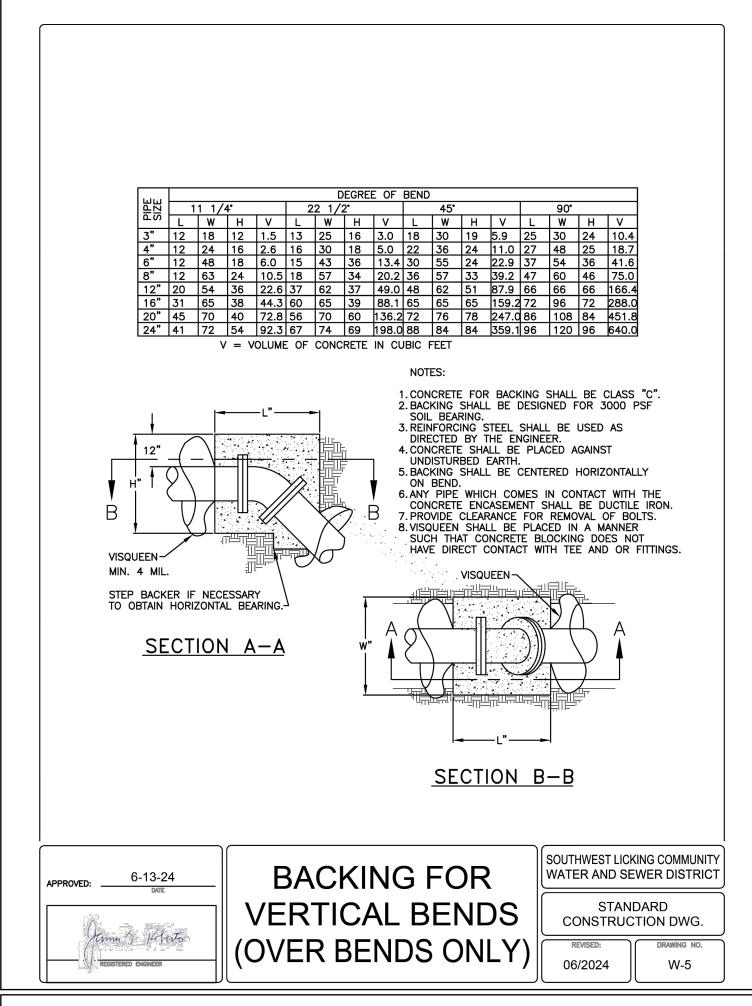


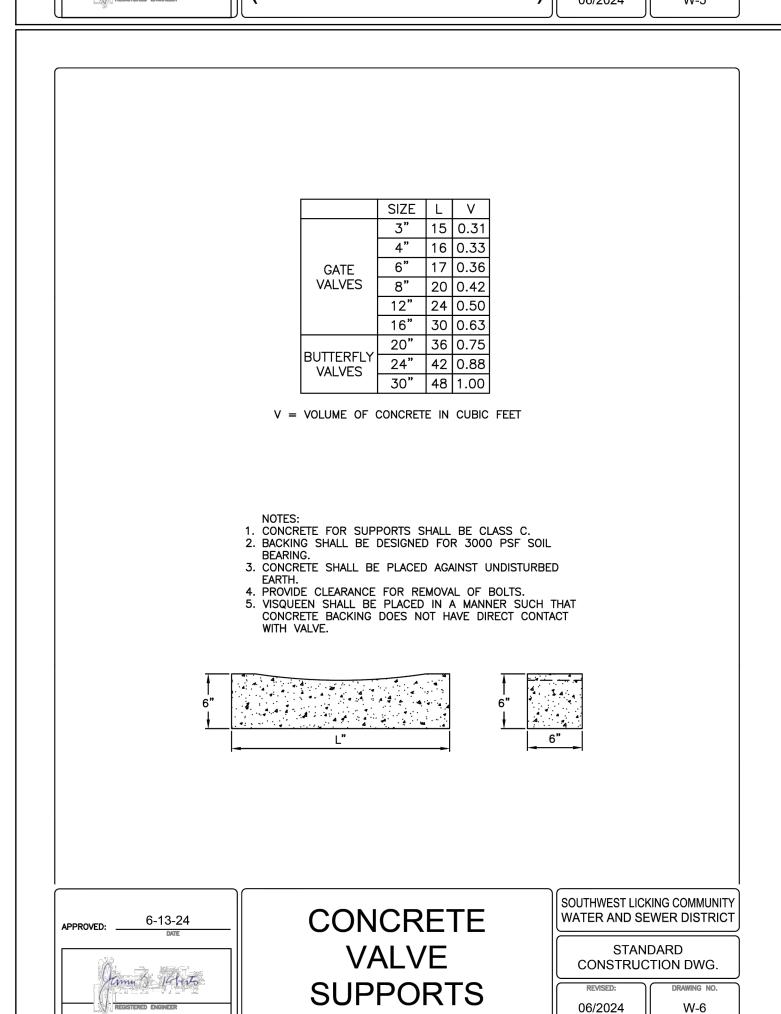


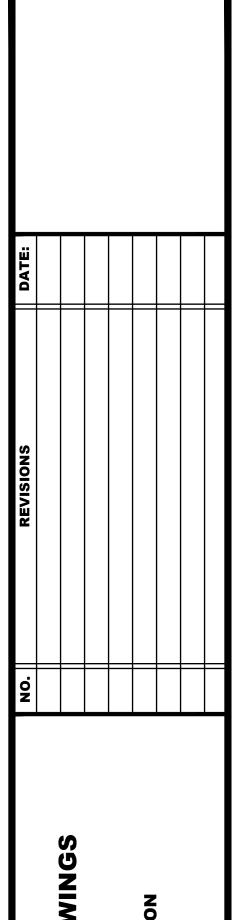












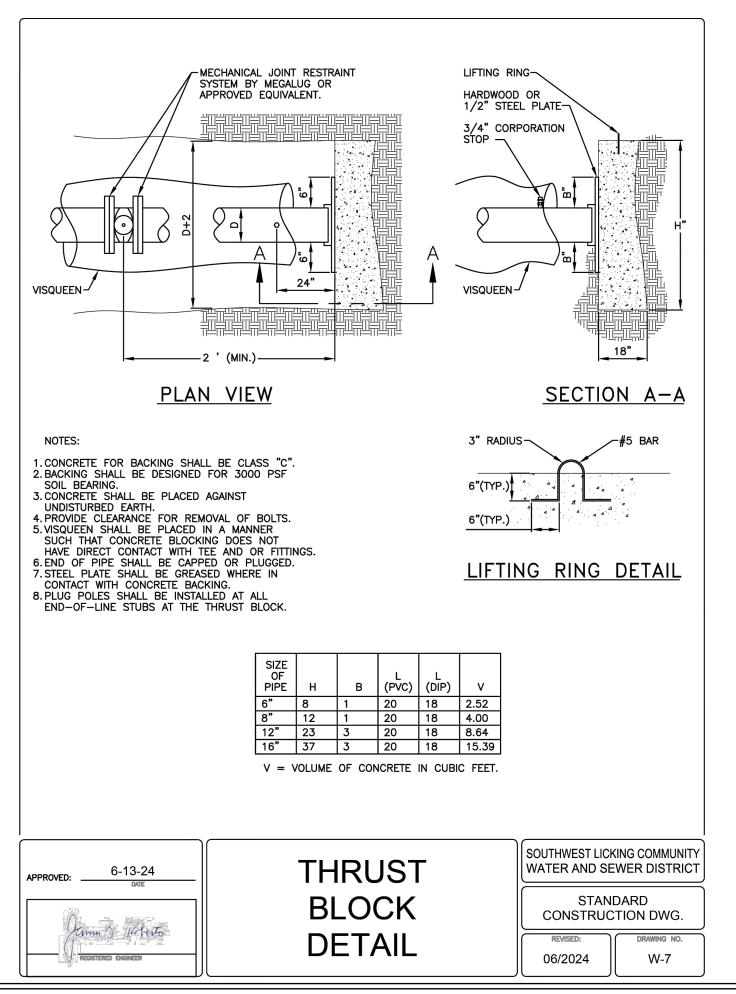
DISTRICT STANDARD CONSTRUCTION DRAN

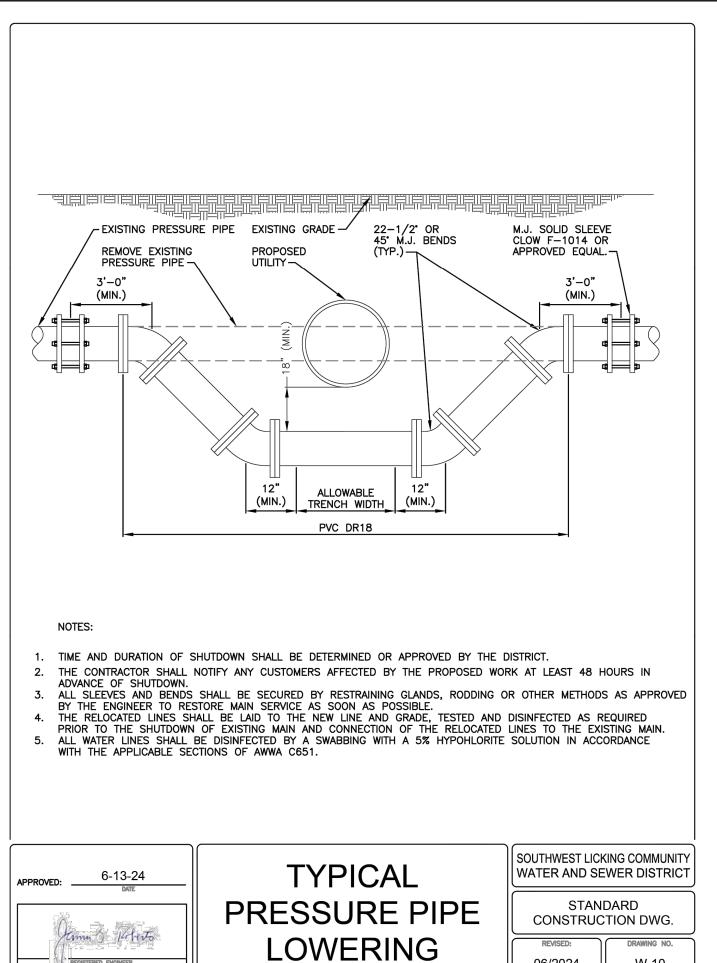
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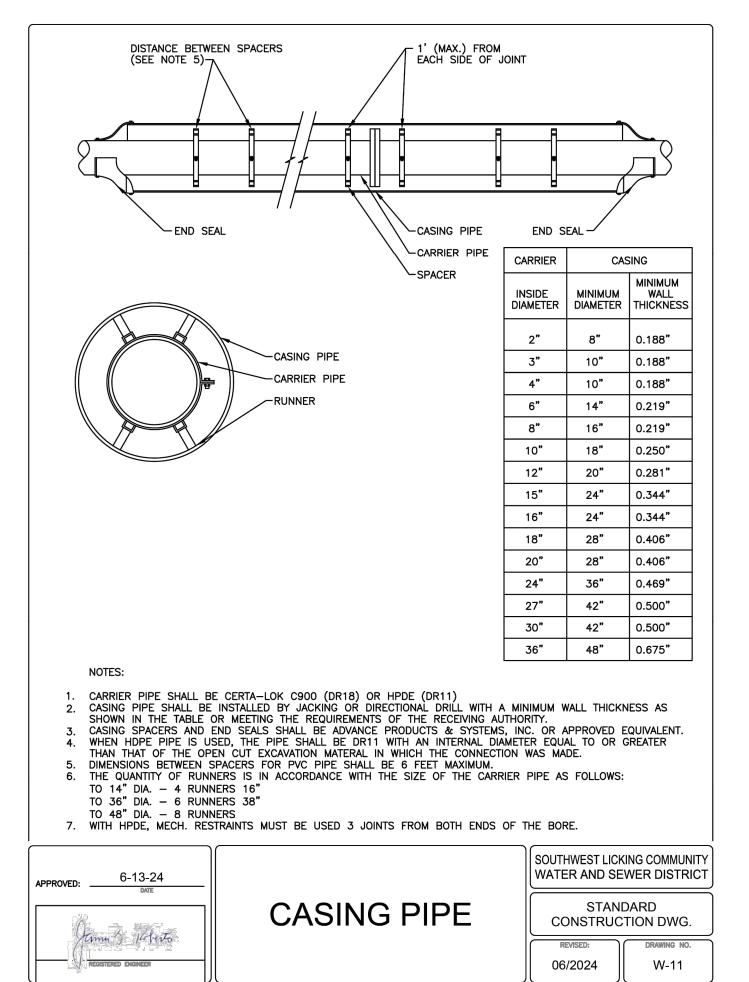
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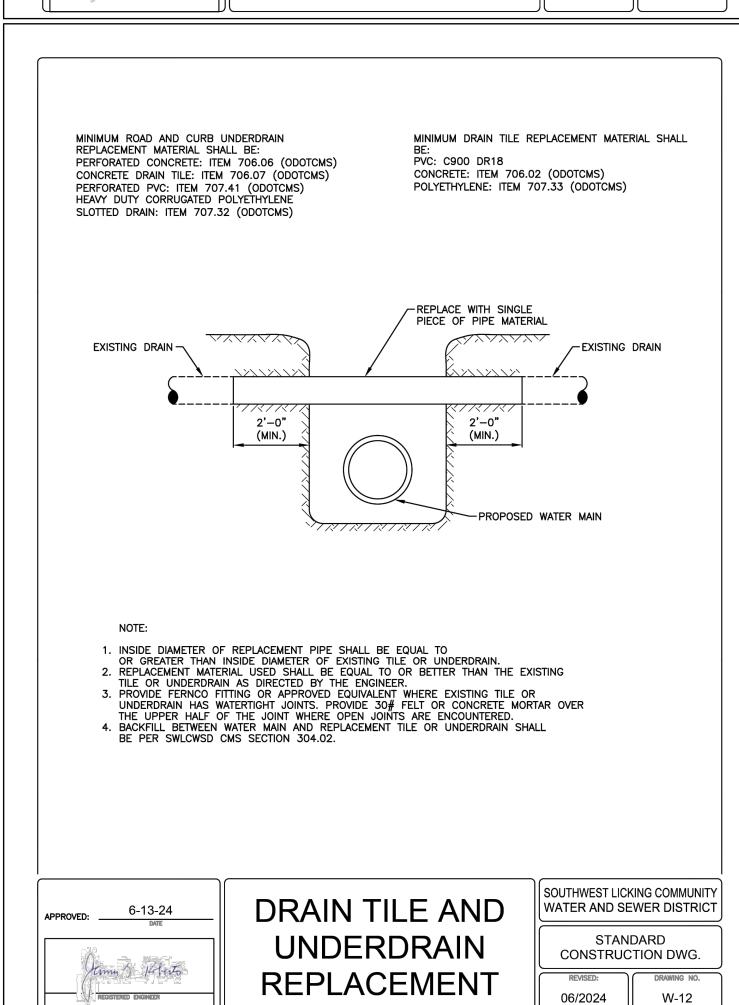


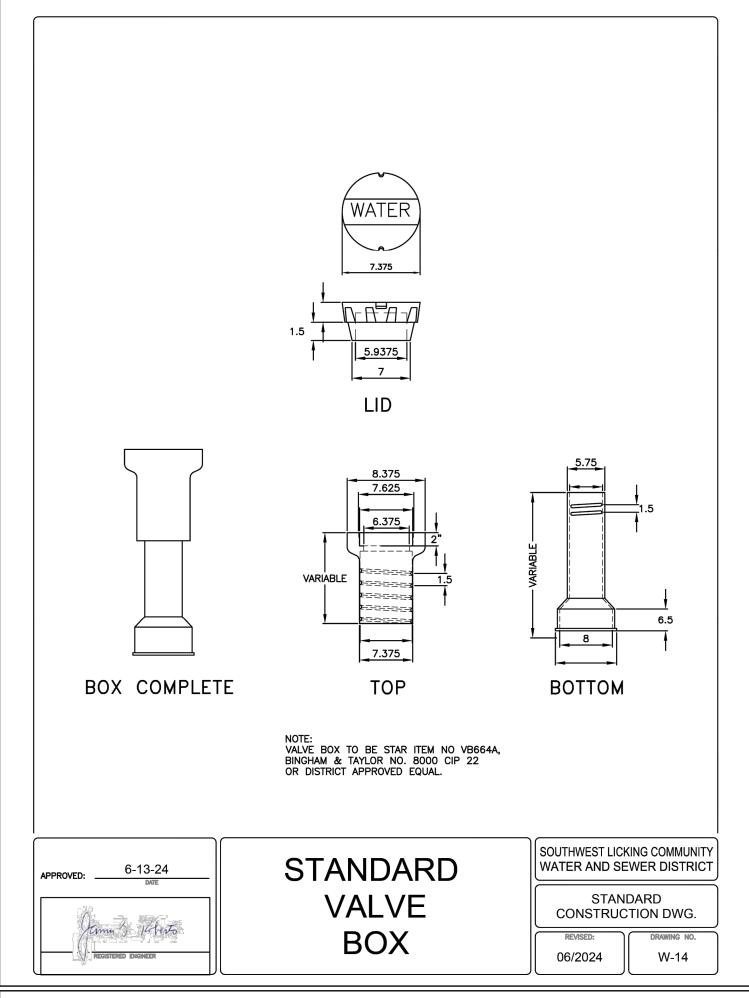
SHEET **12**

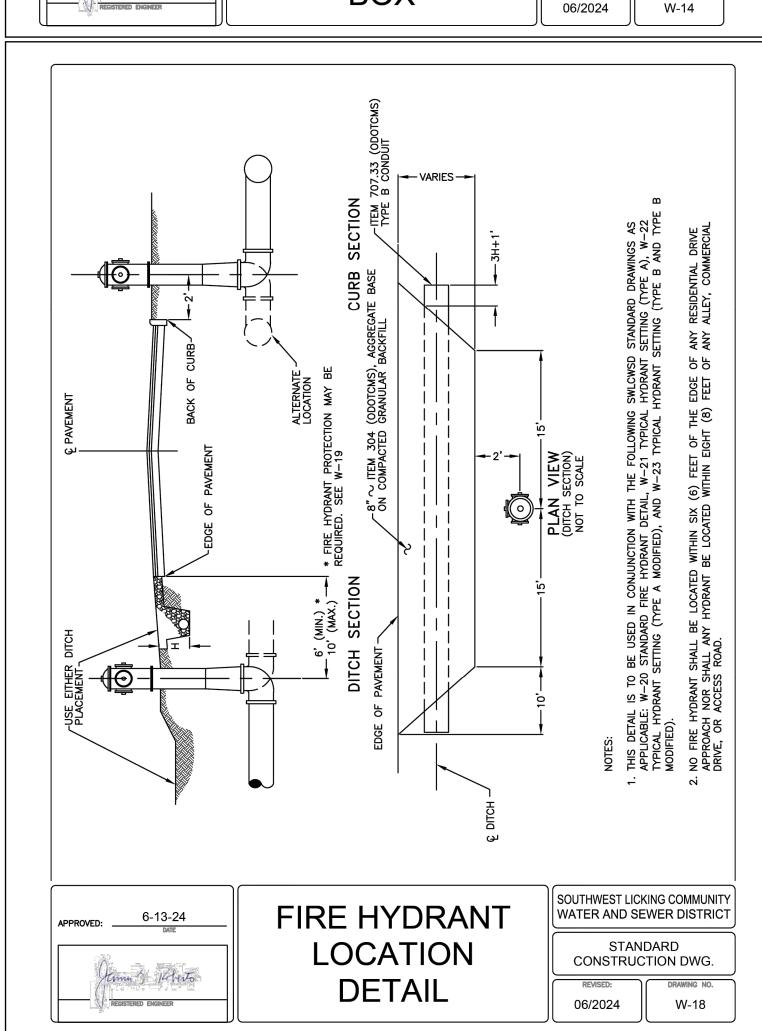


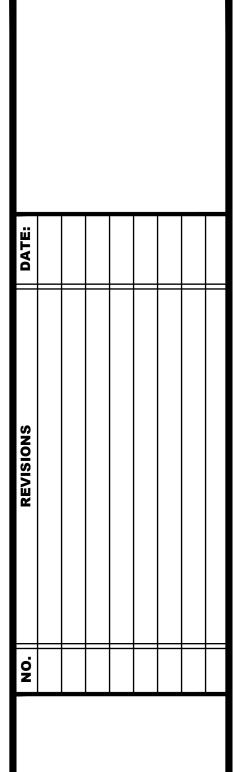












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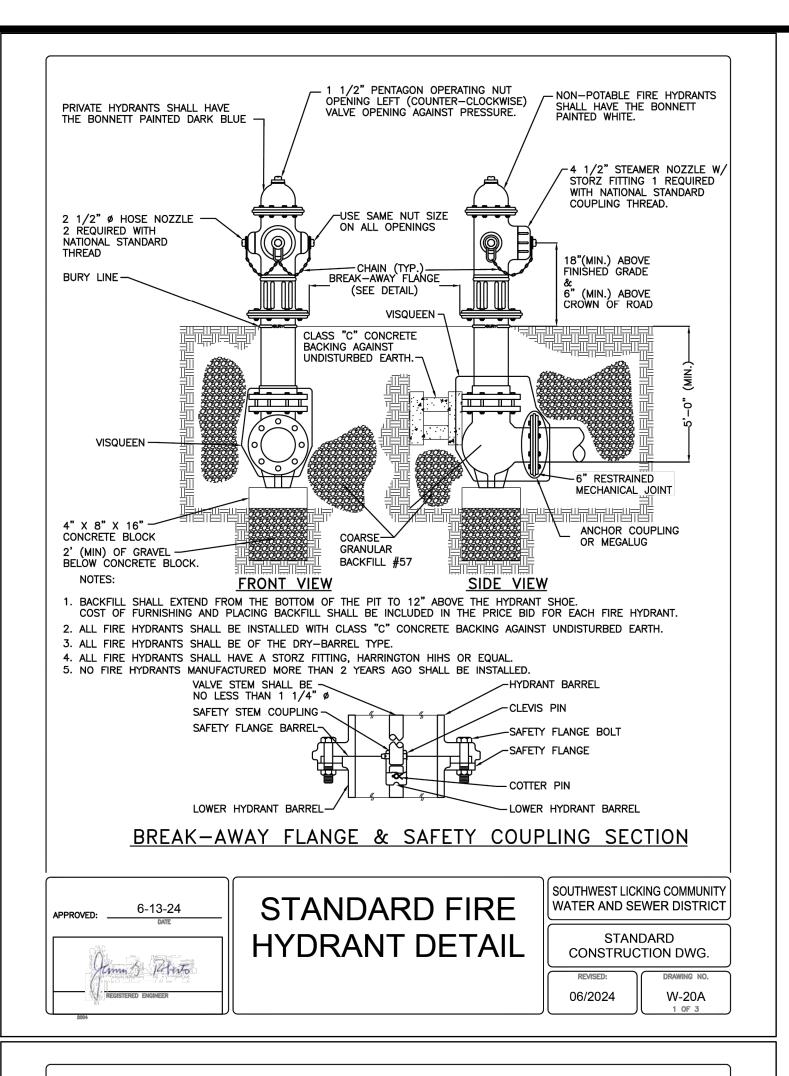
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SHEET **13**

06/2024

W-10





TYPE OF HYDRANT: THE HYDRANT SHALL BE THE POST TYPE TRAFFIC MODEL MADE OF CAST IRON AS SHOWN HEREON, AMERICAN FLOW CONTROL MK 73-5 CLOW MEDALLION, MUELLER SUPER CENTURION 250 MODEL A-421. IT SHALL HAVE A BREAKING CONNECTION THAT PREVENTS LOSS OF WATER WHEN THE UPPER AND LOWER SECTIONS ARE SEPARATED BY A SMASHING IMPACT. THE HYDRANT SHALL BE OF THE COMPRESSION TYPE WITH THE VALVE OPENING IN A COUNTERCLOCKWISE DIRECTION AGAINST THE PRESSURE AND CLOSING WITH THE PRESSURE. THE VALVE END OF THE STEM OR VALVE ROD SHALL BE SO CONSTRUCTED AS TO ELIMINATE CONTACT OF DISSIMILAR METALS IN THE PRESENCE OF MOISTURE.

THE STEM OR VALVE ROD SHALL BE CONSTRUCTED IN ONE CONTINUOUS LENGTH FROM THE VALVE TO THE BREAKING COUPLING OR TO THE BOTTOM OF THE EXTENSION PIECE WHERE EXTENSIONS ARE REQUIRED. THE STEM OR VALVE ROD BETWEEN THE VALVE AND OPERATING NUT SHALL BE MADE OF STEEL STOCK AND HAVE A 1 1/4 INCH MINIMUM DIAMETER AFTER MACHINING. THE BREAKING COUPLING SHALL FIT OVER THE VALVE ROD AND BE LOCATED AT THE PROPER POINT TO CONFORM TO THE BREAKING CONNECTION IN THE STANDPIPE.

THE BARREL SHALL HAVE AN AREA OF NOT LESS THAN 120 PERCENT OF THE VALVE OPENING. THE TYPE OF VALVE SEAL SHALL BE RUBBER WITH

THE DIAMETER OF THE PORT IN THE SEAL RING BEING A MINIMUM OF 4 1/4 INCH.

ALL INTERIOR WORKING PARTS OF THE HYDRANT INCLUDING THE VALVE AND VALVE SEAT SHALL BE SUCH THAT THEY CAN BE REMOVED THROUGH THE TOP OF THE STANDPIPE WITHOUT EXCAVATION. THE UPPER SECTION

OF THE STANDPIPE ABOVE THE GROUND LINE SHALL BE ADJUSTABLE SO

THAT THE NOZZLES CAN BE ROTATED TO ANY DESIRED POSITION.

REFERENCE SPECIFICATIONS: ALL FIRE HYDRANTS SHALL CONFORM
TO THE LATEST AMERICAN WATER WORKS ASSOCIATION
SPECIFICATIONS C502, THE REQUIREMENTS OF THE DISTRICT AND
APPLICABLE LOCAL FIRE DEPARTMENT. ALL SPECIFICATIONS SHALL
BE THE LATEST EDITION IN EFFECT ON THE DATE THE
CONSTRUCTION DRAWINGS ARE APPROVED (SIGNED) BY THE

DISTRICT, UNLESS OTHERWISE NOTED.

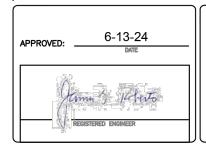
APPROVALS AND CERTIFICATION: THE SUPPLIER OR MANUFACTURER SHALL SUBMIT TO THE DISTRICT SIX (6) COPIES OF THE RESULTS OF CERTIFIED FLOW TESTS RUN BY AN INDEPENDENT TESTING LABORATORY AND SHOP DRAWINGS WITH DIMENSIONS, MATERIALS AND NOMENCLATURE OF PARTS FOR EACH TYPE OR MODEL OF HYDRANT PROPOSED FOR USE IN THE PROJECT AREA.

UPON APPROVAL OF THE ABOVE INFORMATION BY THE DISTRICT, IT SHALL REMAIN ON FILE WITH THE DISTRICT. SUBMISSION OF THE ABOVE MATERIALS WITH EACH ORDER OF FIRE HYDRANTS IS NOT NECESSARY IF APPROVED MATERIAL IS ALREADY ON FILE. SUBMISSION OF NEW MATERIAL IS REQUIRED WHEN A DEVIATION IN THE PRODUCT, ITS MANUFACTURER, OR THE STANDARDS IS REQUESTED.

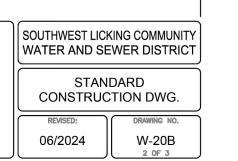
ANY FIRE HYDRANTS DELIVERED TO A PROJECT WHICH FAIL TO CONFORM TO THE APPROVED INFORMATION ON FILE WITH THE

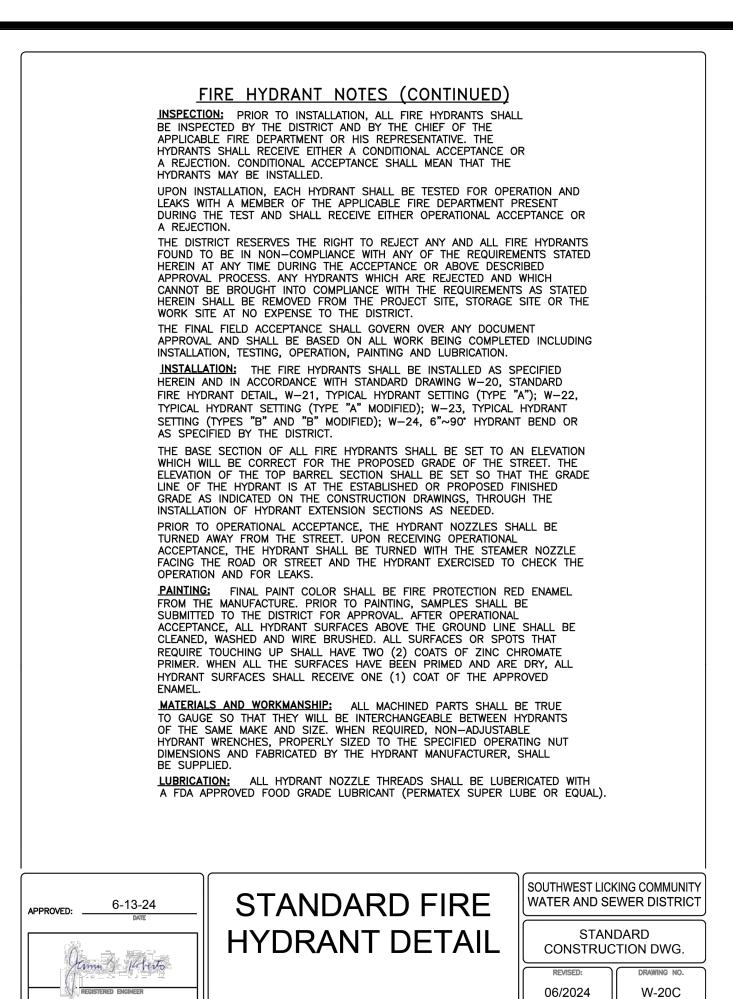
DISTRICT SHALL BE REJECTED.

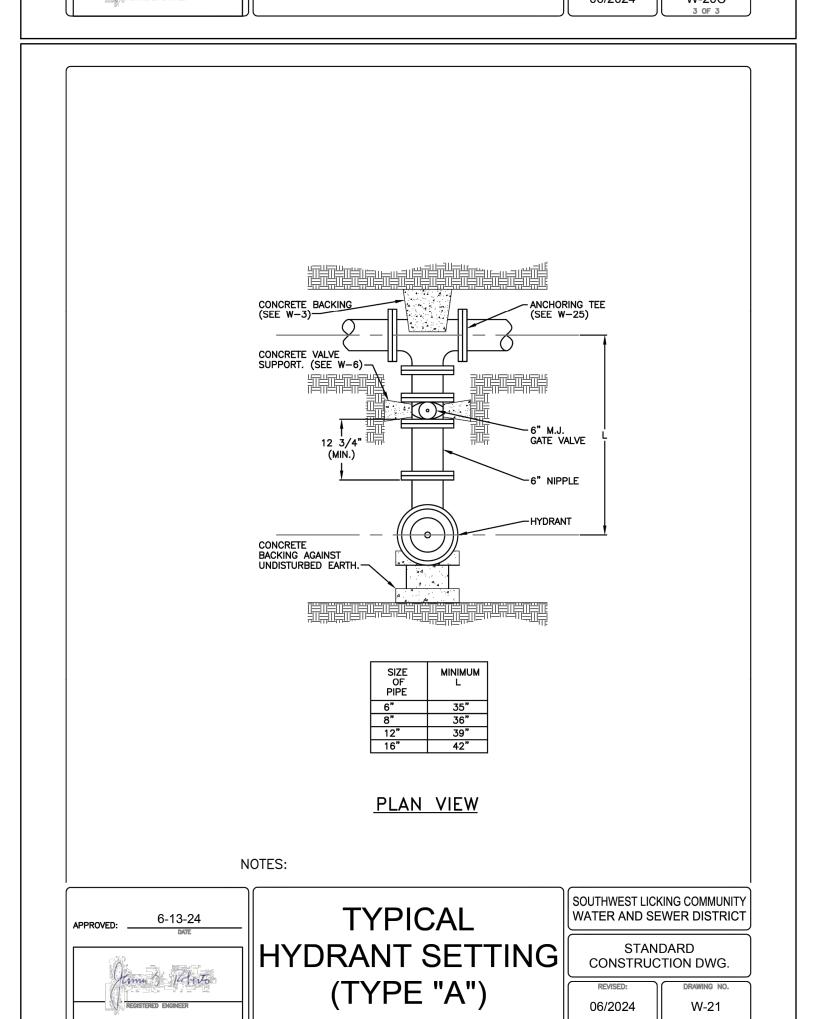
WITH EACH DELIVERY SHIPMENT OF FIRE HYDRANTS, THE HYDRANT MANUFACTURER SHALL CERTIFY THAT THE HYDRANTS CONFORM TO THE INFORMATION APPROVED AND ON FILE WITH THE DISTRICT. THIS CERTIFICATE SHALL INCLUDE THE MODEL OR IDENTIFICATION NUMBERS OF THE HYDRANTS BEING DELIVERED AND THE APPROVAL DATE OF THE INFORMATION ON FILE WITH THE DISTRICT. THIS DOCUMENTATION DOES NOT CONSTITUTE APPROVAL OF FINAL ACCEPTANCE OF THE SPECIFIC HYDRANTS DELIVERED.

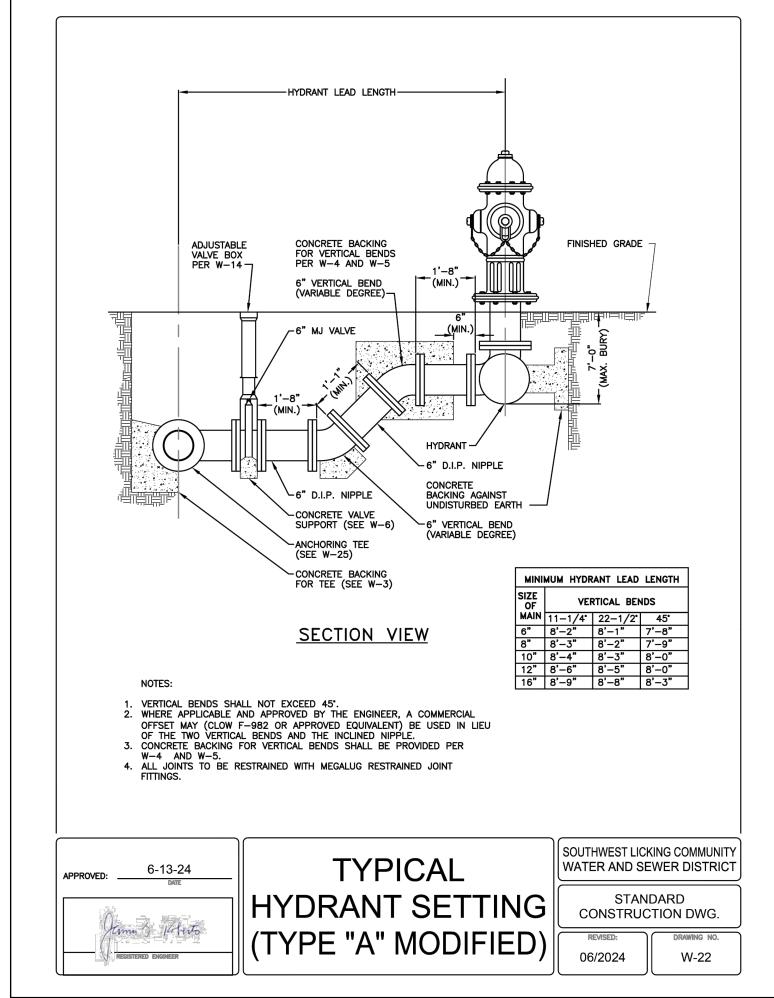


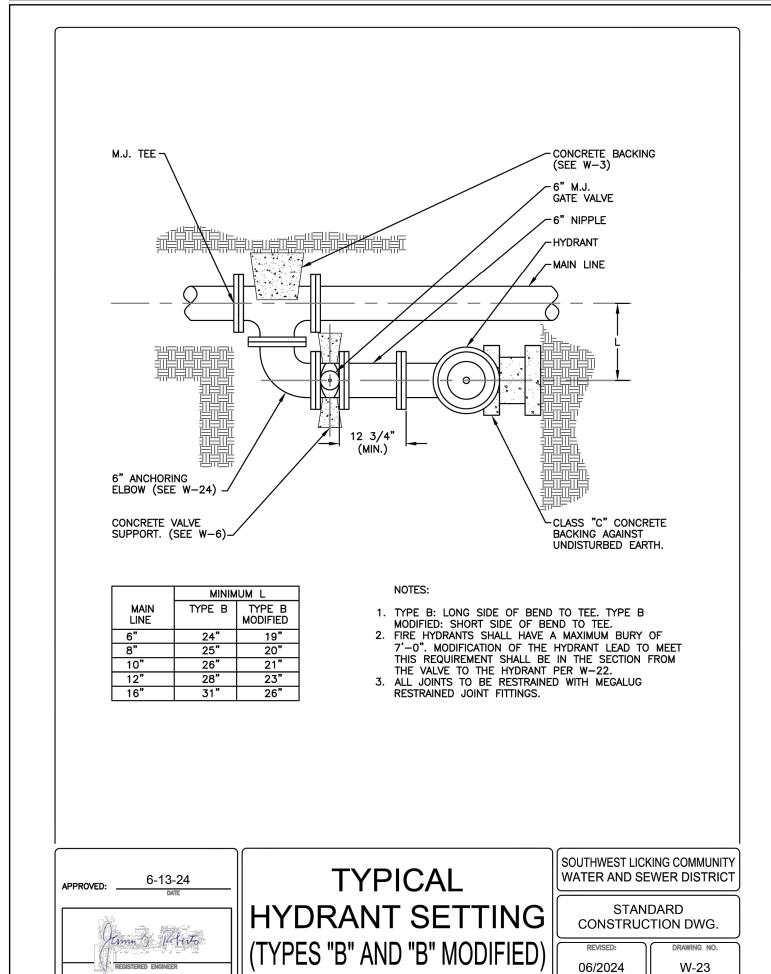
STANDARD FIRE HYDRANT DETAIL

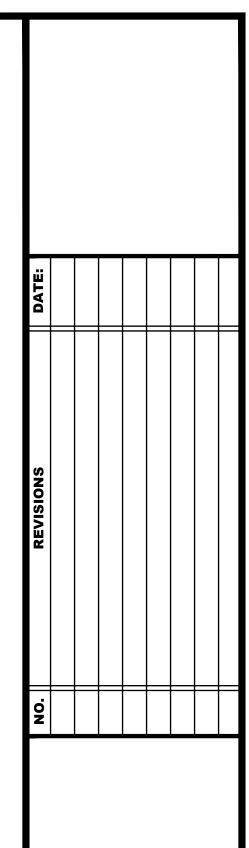












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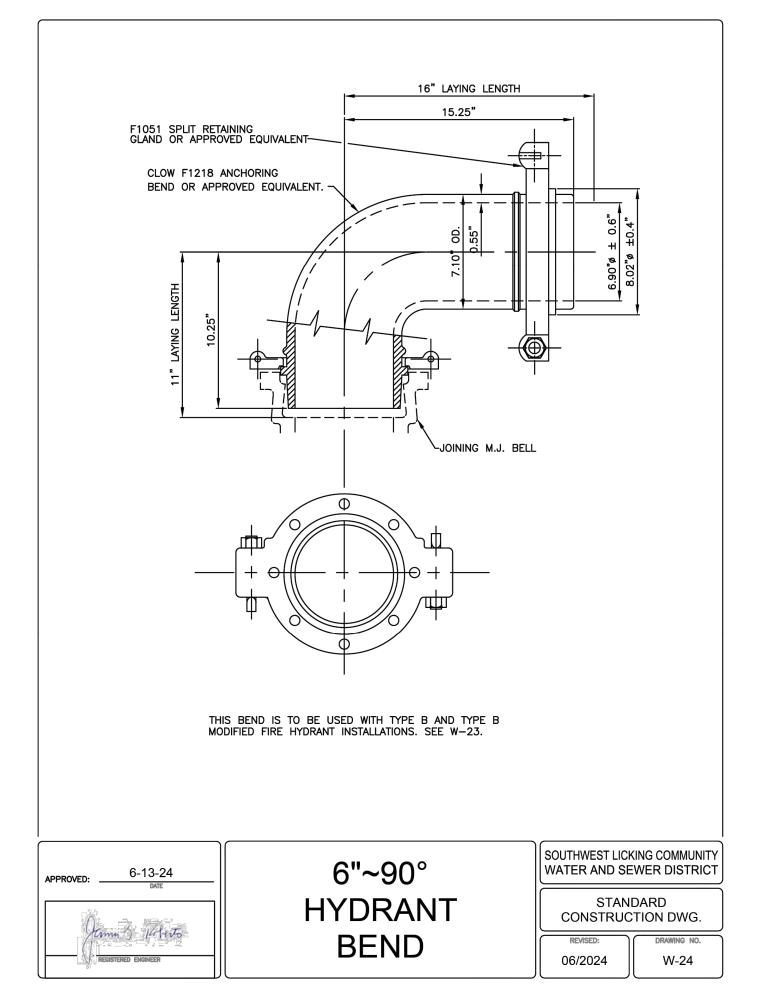
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PROJECT MGR.

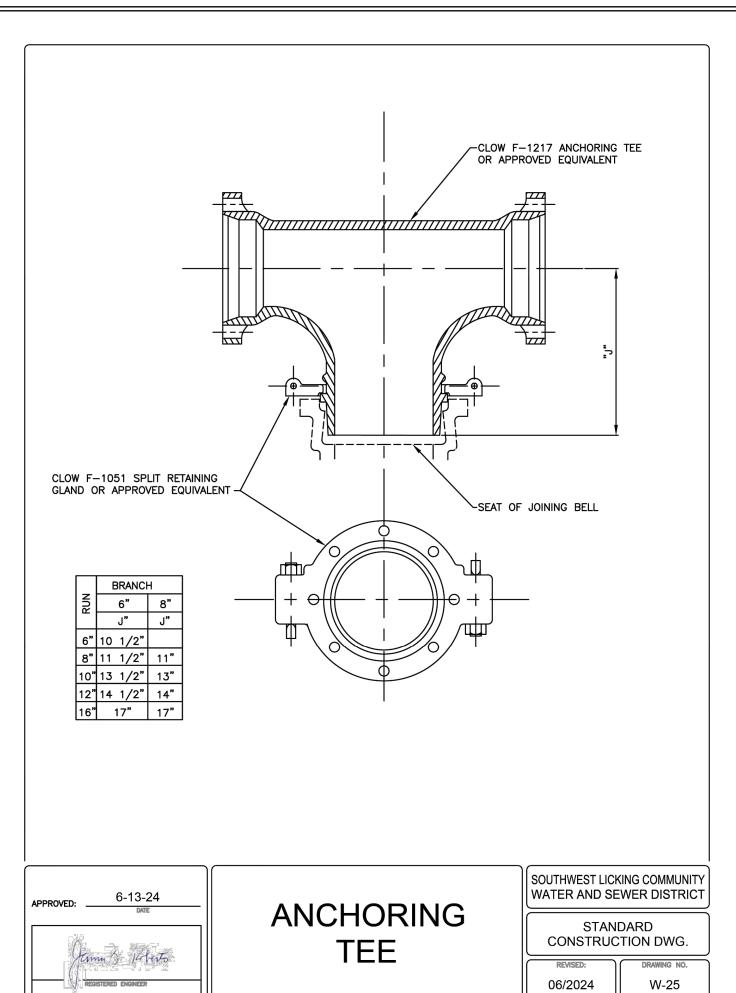
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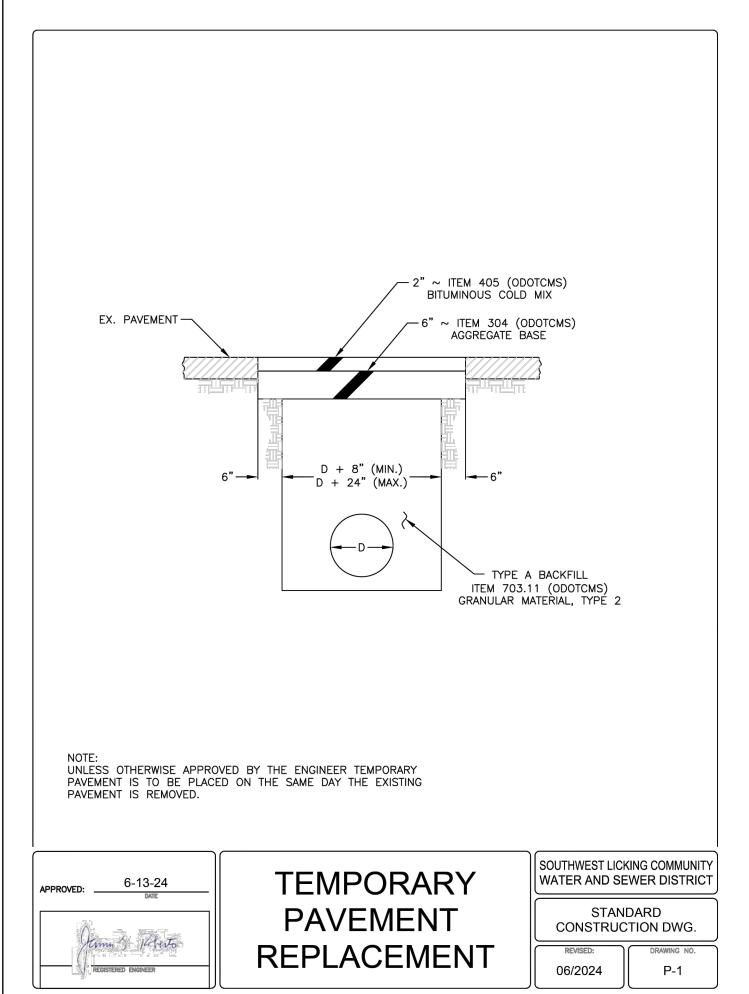
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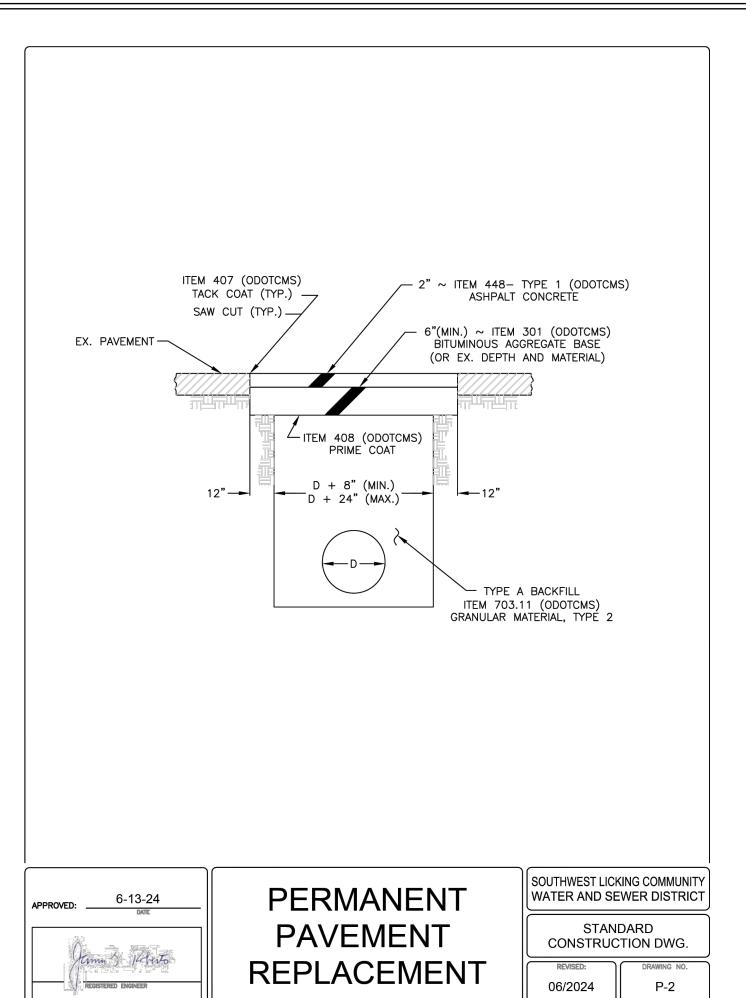
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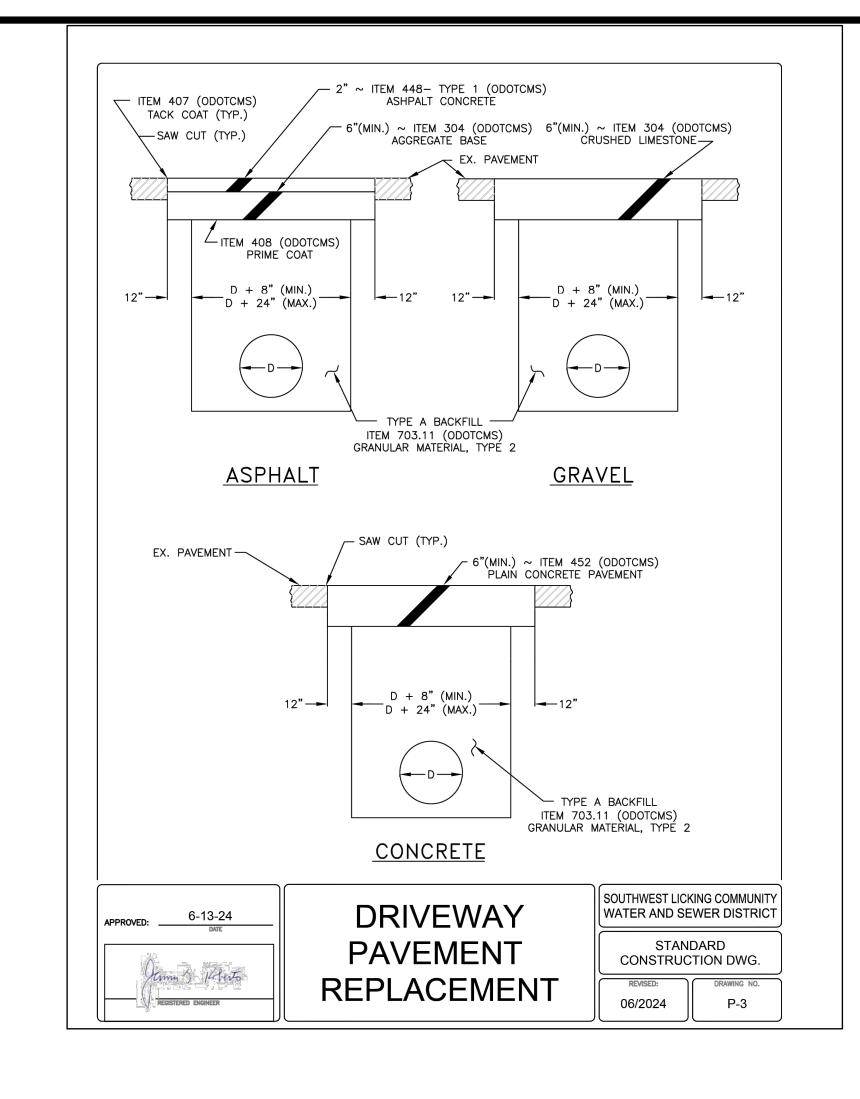
ASSOCIATES

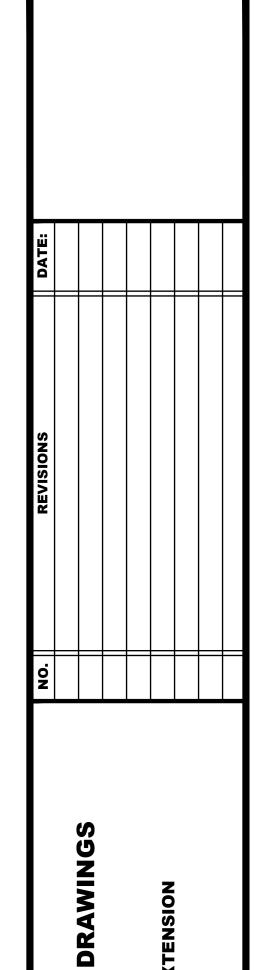












DISTRICT STANDARD CONSTRUCTION DR

JOB NO. 4379.006

PROJECT MGR.



SHEET15

File: S:\COO\4300—4399\4379\006\Drawings\CAD\Civil 3d\SheetsPlan\SWLCWSD_Standard Drawings Sheet.dwg Time: Aug 22, 2025 — 12:13pm